



Planning Commission Work Session Agenda

Date: Thursday, October 2, 2025
Time: 4:30 PM

Location: Bella Vista District Court
2483 Forest Hills Blvd

Planning Commission regular meetings are live streamed and archived for your convenience. View the meeting at its scheduled time or after at:
<https://bit.ly/bvmeetingslive>.

I. Call to Order

II. Roll Call

Members: Daniel Ellis, PE, Chairman; Doug Farner, Vice-Chairman; Gail Klesen, Secretary; Clayton Sedberry, JB Portillo; Jack Wagnon, and Linda Lloyd.

III. Consideration of Minutes

A. Regular Meeting Minutes - September 8, 2025

IV. Unfinished Business

V. New Business

- A. **WVR-2025-61024:** A waiver request on maximum driveway slope requirements for an existing house located at 17 Grantham Drive; Parcel 16-19353-000; Applicant Amithab Mohan Krishna; Presented by Planner Hyatt
- B. **WVR-2025-61502:** A waiver request on distance requirements in between access drives for a proposed driveway servicing a new water tower; Near Highlands Blvd. & Glasgow Road; Parcel 18-12513-009; Presented by Planner Grady
- C. **WVR-2025-61092:** A waiver request on pedestrian accommodation design requirements associated with a preliminary plat 61139 at 102 Fairway Drive; Parcel 16-11099-000; Applicant Linda Lloyd; Presented by Planner Hyatt

- D. **PPL-2025-61139**: A proposed preliminary plat establishing 23 residential lots known as the Owl Commons Subdivision in Bella Vista at 102 Fairway Drive; Parcel 16-11099-000; Applicant Linda Lloyd; Presented by Planner Hyatt

VI. Open Discussion

VII. Announcements

- A. The Board of Zoning Adjustment meeting will be on Monday, October 13, 2025 at 4:30 pm directly followed by the Planning Commission regular meeting.
- B. The City Council Work Session will be Monday, October 20, 2025 at 5:30 pm.
- C. The City Council Regular Meeting will be on Monday, October 27, 2025 at 6:00 pm.
- D. The Planning Commission Work Session will be Thursday, October 30, 2025 at 4:30 pm.

VIII. Adjournment



**Planning Commission
Regular Meeting Minutes**

September 8, 2025
2483 Forest Hills Blvd

I. CALL TO ORDER

Chairman Ellis called the meeting to order at 5:04 PM

II. ROLL CALL

Members: Daniel Ellis, PE, Chairman; Doug Farner, Vice-Chairman; Gail Klesen, Secretary; Clayton Sedberry; JB Portillo; Jack Wagnon, and Linda Lloyd.

Member(s) Absent: None.

III. CONSIDERATION OF MINUTES

A. *Regular Meeting Minutes – August 11, 2025*

On a motion by Mr. Farner and a second by Ms. Portillo, the August 11, 2025 minutes were approved by a voice vote.

IV. PUBLIC INPUT

- A. Chairman Ellis opened the public input session.
- B. Chairman Ellis closed the public input session.

V. UNFINISHED BUSINESS

A. WVR-2025-59726(A): A waiver request on required right-of-way dedication associated with PLA 59124 near 411 Town Center (East); Parcel 16-40168-000; Applicant Crafton Tull; Presented by Planner Grady.

1. Planner Grady presented the staff report in the packet.
2. A discussion was held regarding the request.
3. Applicant Evan McMahan was present to answer questions and reiterate hardship.
4. A motion to approve the waiver was made by Ms. Portillo and a second was made by Ms. Lloyd.

Doug Farner – Y
JB Portillo – Y
Jack Wagnon – Y
Linda Lloyd - Y
Gail Klesen – Y
Clayton Sedberry – Y
Daniel Ellis – Abstain

Motion approved with six yes votes. (6-0)

B. PLA-2025-59124: A Property line adjustment proposal located near 411 Town Center (East); Parcel 16-40168-000; Applicant Craffton Tull; Presented by Planner Grady.

1. Planner Grady presented the staff report alongside item A.
2. A discussion was held regarding the request.
3. Applicant Evan McMahon was present to reiterate hardship and answer questions.
4. A motion to approve the property line adjustment was made by Ms. Klesen and a second was made by Mr. Sedberry.

JB Portillo - Y
Jack Wagnon – Y
Linda Lloyd – Y
Gail Klesen – Y
Clayton Sedberry - Y
Doug Farner - Y
Daniel Ellis - Abstain

Motion approved with six yes votes. (6-0)

C. WVR-2025-59726(B): A waiver request of retaining wall setback requirements for a proposed retaining wall located near 411 Town Center (East); Parcel 16-40168-000; Applicant Craffton Tull; Presented by Senior Planner Hyatt.

1. Senior Planner Hyatt presented the staff report in the packet.
2. Discussion was held regarding the proposal.
3. The applicant Evan McMahon was present to answer questions and reiterate hardship.
4. A motion to approve the waiver was made by Ms. Klesen and a second was made by Mr. Farner.

Jack Wagnon – Y
Linda Lloyd – Y
Gail Klesen – Y
Clayton Sedberry – Y
Doug Farner – Y
JB Portillo - Y
Daniel Ellis - Abstain

Motion approved with six yes votes. (6-0)

VI. NEW BUSINESS

A. LSD-2025-57468: A request to extend the approved development permit for the Republic Services Transfer Station for an additional six months at 36 Bella Vista Way; Parcel 16-43396-000; Applicant Dan Koziatek; Presented by Senior Planner Hyatt

1. Senior Planner Hyatt presented the staff report in the packet.
2. Stephen Brown, representing Republic Services was present for questions.
3. A discussion was held regarding the request.
4. A motion to approve the large-scale development extension was made by Mr. Farner and a second was made by Mr. Sedberry.

Linda Lloyd – Y
Gail Klesen – Y
Clayton Sedberry – Y
Doug Farner – Y

JB Portillo - Y
Jack Wagnon - Y
Daniel Ellis - Y

Motion approved with seven yes votes. (7-0)

B. Public Hearing RZN-2025-59940: A request to rezone the subject property from R-E Residential Estate to C-2 Light/Local Commercial near Lancashire Blvd and Oakhill Drive; Parcel 16-43269-000; Applicant Jack Kehoe; Presented by Planner Grady.

1. Planner Grady presented the staff report in the packet.
2. Christopher Brandon was present to answer questions and reiterate intent.
3. A discussion was held regarding the request.
4. Commissioner Ellis opened the public hearing.
5. Darin Callahan, 645 W Lancashire Blvd spoke in opposition of the rezone.
6. Jaclyn Callahan, 645 W Lancashire Blvd spoke in opposition of the rezone.
7. Randy Kizzia, 14892 Oak Hill Rd. spoke in opposition of the rezone.
8. Commissioner Ellis closed the public hearing.
9. A motion to recommend the rezone to city council was made by Ms. Klesen and a second was made by Mr. Sedberry.

Gail Klesen – N
Clayton Sedberry – N
Doug Farner – N
JB Portillo - N
Jack Wagnon - N
Linda Lloyd - Y
Daniel Ellis - N

Motion denied with six no votes and one yes vote. (1-6)

C. WVR-2025-60189: A waiver request of retaining wall setback requirements for an existing retaining wall located at 3 Buckingham Dr; Parcel 16-05904-000; Applicant Zachariah Smith; Presented by Senior Planner Hyatt

1. Senior Planner Hyatt presented the staff report in the packet.
2. Staff recommends approval as long as a septic design be established for the adjacent lot.
3. A discussion was held regarding the request.
4. Zach Smith was present to answer questions and reiterate hardship.
5. A motion to approve the waiver with staff conditions was made by Mr. Sedberry and a second was made by Mr. Wagnon.

Clayton Sedberry – Y
Doug Farner – Y
JB Portillo - Y
Jack Wagnon - Y
Linda Lloyd - Y
Gail Klesen - Y
Daniel Ellis - Y

Motion approved with seven yes votes. (7-0)

D. MSD-2025-60193: A proposed minor subdivision creating four new lots near Copinsay Drive and Kirkwall Drive; Parcel 16-72577-012; Applicant Cooper Communities; Presented by Planner LaRue

1. Planner LaRue presented the staff report in the packet.
2. Kent Burger was present to answer questions.
3. A discussion was held regarding the request.
4. A motion to approve the minor subdivision was made by Mr. Farner and a second was made by Ms. Portillo.

Doug Farner – Y
JB Portillo - Y
Jack Wagnon - Y
Linda Lloyd - Y
Gail Klesen – Y
Clayton Sedberry - Y
Daniel Ellis - Y

Motion approved with seven yes votes. (7-0)

VII. COMMITTEE REPORTS

None.

VIII. OPEN DISCUSSION

A. House placement certification discussion

IX. ANNOUNCEMENTS

A summary of upcoming meetings was announced.

IIX. ADJOURNMENT

Chairman Ellis adjourned the meeting at 5:53 pm.

SUBMITTED BY:

Sarah Costa, Office Manager
City of Bella Vista

APPROVED AND ACCEPTED THIS 13TH DAY OF October 2025:

Daniel Ellis, P.E., Chairman
Bella Vista Planning Commission

Gail Klesen, Secretary
Bella Vista Planning Commission



Meeting Information:
 2483 Forest Hill Blvd
 October 2, 2025 at 4:30 pm
Reviewer:
 Christopher Hyatt
 Senior Planner

Property Description

This property is located in the eastern area of Bella Vista City Limits and is located on Parcel #16-19353-000. Lot 11, Block 1. Lincoln Subdivision.

Regulation

Sec. 107-380(f) regulates maximum driveway slope percentages

Request

The applicant is requesting a waiver of Sec. 107-380(f) for an existing driveway retaining wall in the right of way.

Background

The primary residence under construction is undergoing inspections and at the August 12th Structural – C/O Final inspection, it was discovered that the driveway did not meet the maximum 14% slope. Staff was alerted and the applicant submitted their waiver request on August 26th. The residence is essentially complete, however, a note has been added to the project file that no C of O can be issued until the Planning Commission takes action on the waiver request. Per the applicant's letter, during the final phase of construction for the home, the concrete contractor experienced equipment problems which resulted in the garage elevation being lower than the approved plan. The result was a driveway with a slope of 21.4% downhill for the first 20 feet. The last eight feet slopes upward into the garage. 107-380(f) allows a maximum slope of 14%.

Grantham Drive is a non-classified residential street per the Master Street Plan that experiences low to minimal traffic. Per the Future Land Use Map, the home is within a low-density neighborhood area.

Public Comment

None at the time of this report.

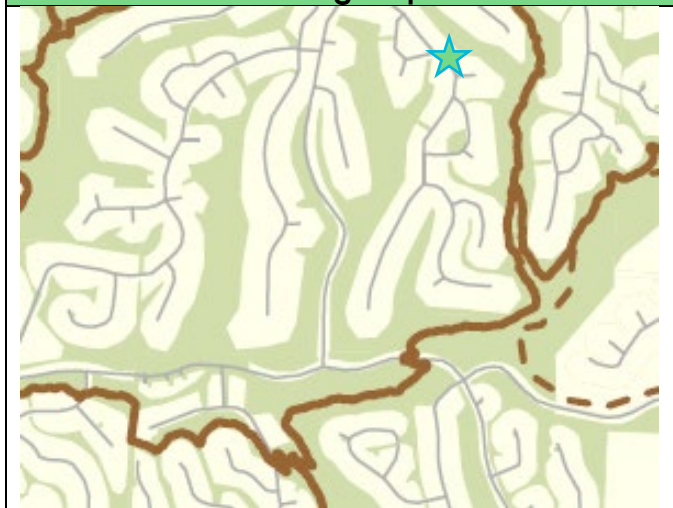
Outstanding Technical Comments

None at the time of this report.

Project Number	WVR-2025-61024
Applicant	Krishna Amithab Mohan
Address/Location	17 Grantham Drive
Current Zoning	R-1 Single Family Residential
Site Area	+/- 0.27 acres
Nature of Request	Seeking a waiver of Sec. 107-380(f).



Zoning Map



Future Land Use Plan

Sec. 107-35 Waiver Review Requirements

When, by the strict interpretation of Chapter 107, an applicant incurs undue restrictions on the physical property to be subdivided or developed, a waiver for such requirements may be granted by the planning commission. **Under no circumstance** should a waiver be granted because of a personal hardship or for personal or emotional reasons. Waivers shall not be granted based strictly on financial hardship. A waiver is determined by the strict interpretation and enforcement of the rules and regulations upon a given piece of property to be subdivided or developed. No waiver shall be granted unless the planning commission finds all of the following:

1. That there are special circumstances or conditions affecting the land involved such that the strict application of the provision of these regulations would deprive the applicant of the reasonable use of this land.
2. That the waiver is necessary for the preservation and enjoyment of a substantial property right of the applicant.
3. That the granting of the waiver will not be detrimental to the public health, safety and welfare or injurious to other property in the area.
4. That the granting of the waiver will not have the effect of preventing the orderly subdivision of other land in the area in accord with the provision of these regulations.

Four affirmative votes of the planning commission's authorized membership shall be necessary for the passage of any waiver.

Sec. 107-35 Waiver Review Procedures

- Under exceptional circumstances, the planning commission may grant waivers at the time of final plat approval.
- In granting the waiver, the planning commission shall prescribe any conditions that it deems necessary to or desirable in the public interest.
- In considering the petition for a waiver, the planning commission shall take into account the nature of the proposed use of land involved, existing uses of land in the area, proximity to public utilities, the number of persons who will reside or work in the proposed subdivision, and the probable effect of such waiver upon traffic conditions and upon the public health, safety and general welfare in the vicinity.
- The findings of the planning commission, together with the specific facts upon which findings are based, shall be incorporated into the official minutes of the planning commission meetings at which such waiver is granted. Waivers may be granted only when in harmony with the general purpose and intent of these regulations.

Request for Waiver Due to Driveway Slope Variation

Project Information

- **To:** City of Bella Vista Planning and Commission
- **From:** Henry Guilarte, GM Homes LLC
- **Project Address:** 17 Grantham Dr, Bella Vista, AR

Summary of Issue

During the final phase of construction, our concrete contractor experienced equipment problems while working on the garage. This resulted in the garage elevation being lower than what was initially approved. To address this issue and prevent water from entering the house, we adjusted the driveway slope. The new slope configuration involves a 21.4% grade over the first 20 feet of the garage, followed by a rise of one foot over the last eight feet to ensure proper drainage.

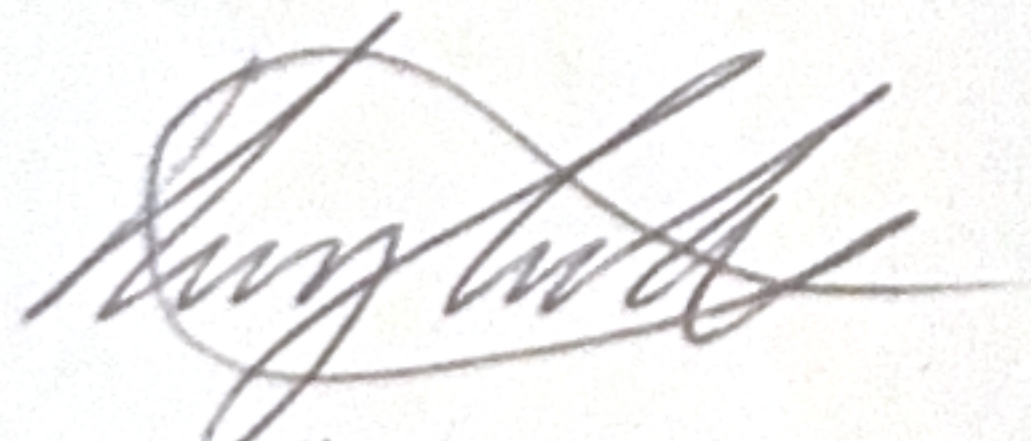
Impact on Adjacent Properties

This adjustment to the driveway slope affects only our property and does not impact neighboring properties or interfere with the orderly subdivision of adjacent lots. This is a specific, isolated issue related solely to our project.

Request for Waiver

We respectfully request that the Planning and Commission Department of Bella Vista waive Chapter 107 of the City Code (2025 Update) and the Arkansas Administrative Code §234.02.22-001 Rule 017. Granting this waiver will allow us to obtain the Certificate of Occupancy so that the property owners may take possession of the home.

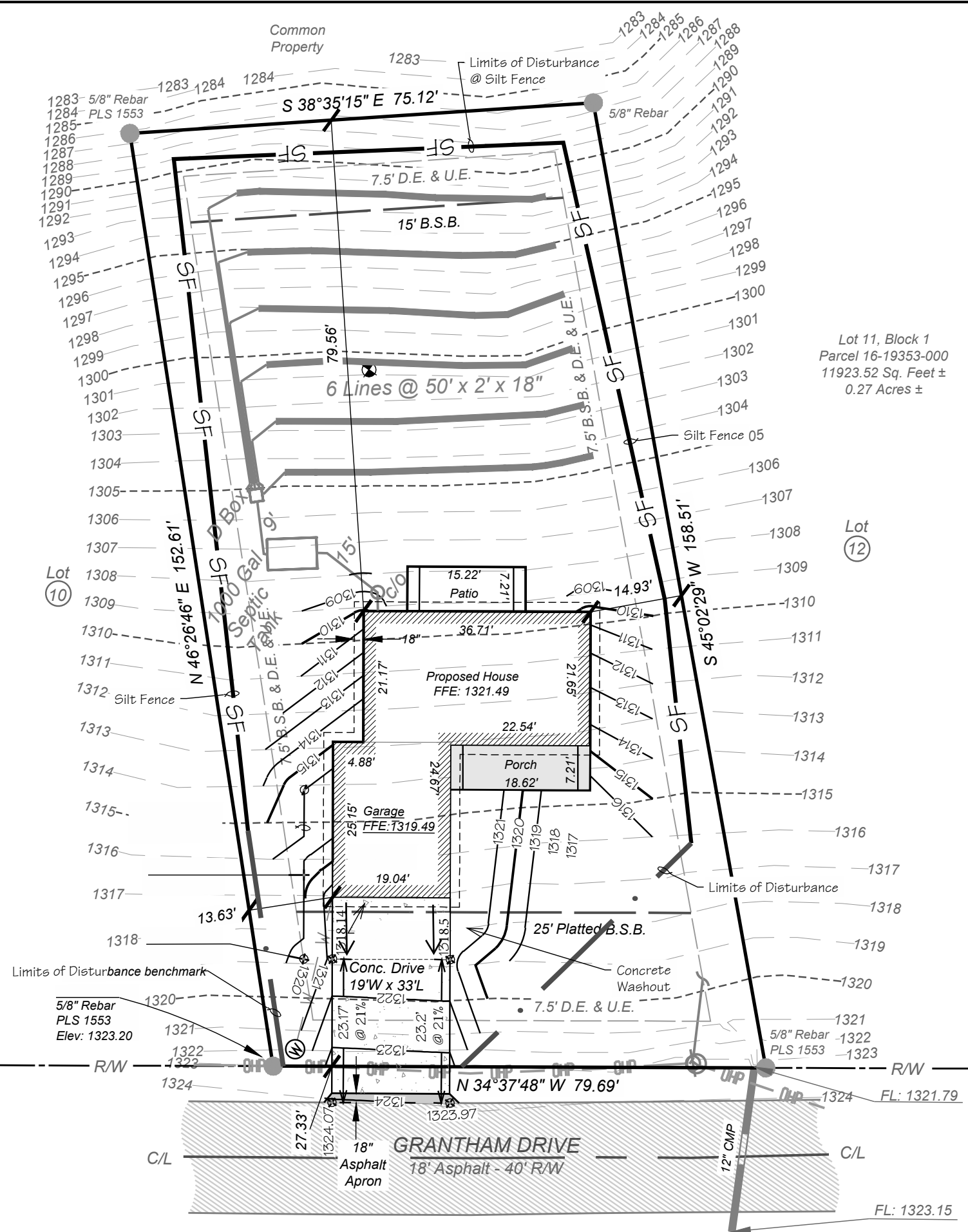
Thank you for your consideration.



Henry Guilarte

GM Homes LLC

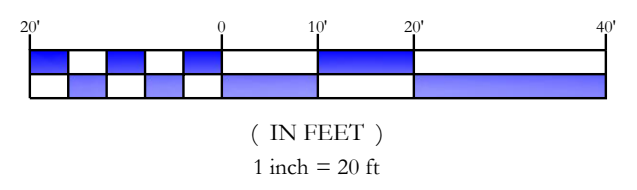
p:\24-6821 bella vista\civil drawings - Plotted on 12/20/2024 4:26:10 PM @ a scale of 1:1 to autocad pdf (smallest file).pc3 by Aaron Long



Site Information:

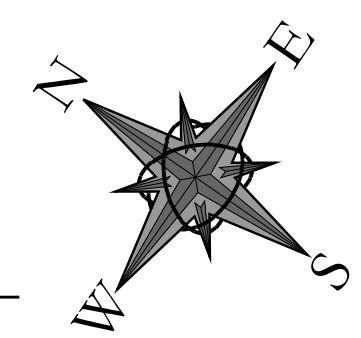
Benton County Assessor Parcel Number: 16-19353-000
 Benton County 911 Admin Assigned Address: 19 Grantham Drive, Bella Vista, 72714
 Property Information: Lot 11 Block 1 Lincoln Subdivision
 Property Owner Information: Real Properties GM
 Zoning Classification: R-1 Residential, Single Family
 Zoning Setbacks: 20 ft (Front)*
 15 ft (Side parallel to ROW)
 7.5 ft (Side)*
 15 ft (Rear)*
 *=Unless otherwise stated on the Subdivision Plat
 Total Acreage: 0.27 ac

GRAPHIC SCALE



Proposed Features:

Feature	Description
	Property Line
	Right-Of-Way Line
	Easement / Setback Line
	Street / Drive Centerline
	Concrete Pavement
	Patio
	Water Line
	Limits of Disturbance
	Silt Fence
	Water Meter



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 Environmental

5102 S. Pinnacle Hills Pkwy Office: (479) 443-4506
 Rogers, Arkansas 72758 www.BLEWINC.com

Certificate of Authorization No 1534

DRAWN BY: A. Long	DATE: 2024-12-20	JOB NUMBER: 24-6821
DRAWING NAME: 24-6821 Civil 002.dwg		







Meeting Information:

2483 Forest Hill Blvd
 October 2, 2025 at 4:30pm

Reviewer:

Jessica Grady, CNU-A
 Planning Technician

Property Description

This property is located in the west area of Bella Vista along Highland Blvd. on the edge of the city limits. It is located on Parcel #18-12513-009, rural subdivision 27-21-32.

Regulation

Sec. 107-317 access management regarding minimum separation distances between access drives.

Request

The applicant is requesting a waiver of Sec. 107-317 for a 200' minimum separation requirement between driveways. Their proposed driveway placement falls within approximately 100' (center of line to center of line) to the Carrol Electric service drive.

Background

The POA Water Department is working on a new site for a 500,000-gallon tank. The subject property falls within Benton County but due to legislative changes to the extra territorial jurisdiction, the property no longer falls within the Bella Vista Planning area. However, a review is still required by Bella Vista Planning due to the access drive falling within the Bella Vista city limits. The purpose of this review is to examine the access drive.

The development property has no frontage along Highlands Blvd., which restricts driveway placement options, and makes full compliance with the required 200-foot separation distance for a roadway with a posted speed limit of 40-mph unachievable. According to the applicant, the existing access easement from 2023, was negotiated, purchased, and recorded prior to the current development design which represents the only feasible point of vehicular entry. Relocating this access would require significant site reconfiguration and the acquisition of additional property. The proposed access drive will utilize the existing easement and will not hinder the public health, safety and welfare or be injurious to future development of surrounding land.

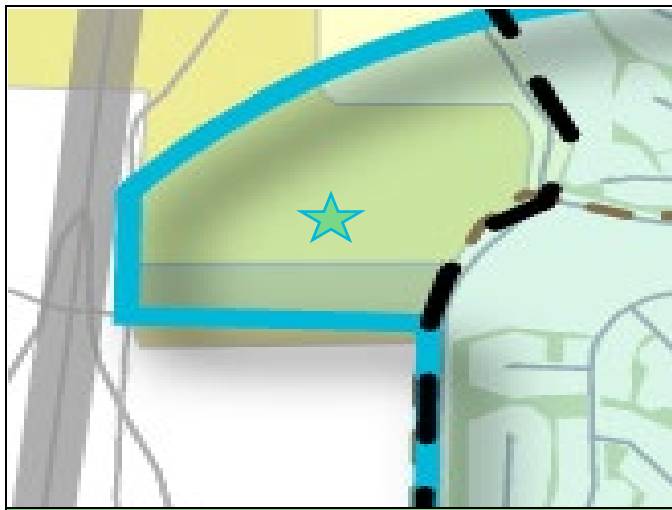
Public Comment

None at the time of this report.

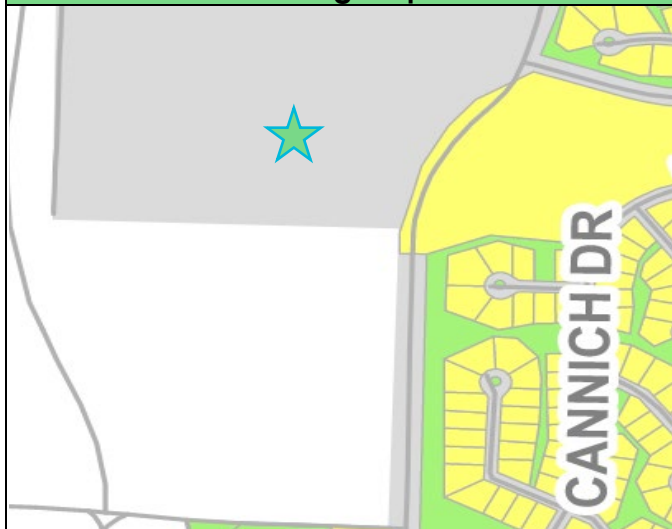
Outstanding Technical Comments

None at the time of this report.

Project Number	WVR-2025-61502
Applicant	Charlie Holt
Address/Location	Highlands Blvd.
Current Zoning	Benton County
Site Area	2.5 acres
Nature of Request	Seeking a waiver of Sec. 107-317.



Zoning Map



Future Land Use Plan

Sec. 107-35 Waiver Review Requirements

When, by the strict interpretation of Chapter 107, an applicant incurs undue restrictions on the physical property to be subdivided or developed, a waiver for such requirements may be granted by the planning commission. **Under no circumstance** should a waiver be granted because of a personal hardship or for personal or emotional reasons. Waivers shall not be granted based strictly on financial hardship. A waiver is determined by the strict interpretation and enforcement of the rules and regulations upon a given piece of property to be subdivided or developed. No waiver shall be granted unless the planning commission finds all of the following:

1. That there are special circumstances or conditions affecting the land involved such that the strict application of the provision of these regulations would deprive the applicant of the reasonable use of this land.
2. That the waiver is necessary for the preservation and enjoyment of a substantial property right of the applicant.
3. That the granting of the waiver will not be detrimental to the public health, safety and welfare or injurious to other property in the area.
4. That the granting of the waiver will not have the effect of preventing the orderly subdivision of other land in the area in accord with the provision of these regulations.

Four affirmative votes of the planning commission's authorized membership shall be necessary for the passage of any waiver.

Sec. 107-35 Waiver Review Procedures

- Under exceptional circumstances, the planning commission may grant waivers at the time of final plat approval.
- In granting the waiver, the planning commission shall prescribe any conditions that it deems necessary to or desirable in the public interest.
- In considering the petition for a waiver, the planning commission shall take into account the nature of the proposed use of land involved, existing uses of land in the area, proximity to public utilities, the number of persons who will reside or work in the proposed subdivision, and the probable effect of such waiver upon traffic conditions and upon the public health, safety and general welfare in the vicinity.
- The findings of the planning commission, together with the specific facts upon which findings are based, shall be incorporated into the official minutes of the planning commission meetings at which such waiver is granted. Waivers may be granted only when in harmony with the general purpose and intent of these regulations.



Water Utility and Billing Department
51 Huntley Lane
Bella Vista, Arkansas 72715
Phone: (479) 855-8000

August 25th, 2025

Bella Vista Planning Commission
101 Town Center
P.O. Box 5655
Bella Vista, AR 72714

RE: Waiver Request – Sec. 107-317. Access Management

Dear Members of the Bella Vista Planning Commission,

I am writing to formally request a waiver from the requirements of **Sec. 107-317 – Access Management** of the Bella Vista Municipal Code, specifically the provisions regarding **minimum separation distances between access drives** on major arterial roadways.

Reason for Waiver Request

The waiver is necessary because the subject property's only feasible vehicular access is via an existing access easement to a previously acquired property. This easement has already been purchased and recorded. Additionally, the property line adjacent to Highlands Boulevard measures less than 200 feet in total length, making it physically impossible to meet the 200-foot minimum separation requirement between the proposed driveway location and existing driveways, as stipulated for a roadway with a posted speed limit of 40 mph.

Special Conditions Affecting the Property

The special conditions are:

1. **Physical Constraints** – The limited frontage along Highlands Boulevard restricts driveway placement options and precludes full compliance with the required separation distances.
2. **Preexisting Access Arrangement** – The access easement was negotiated, purchased, and finalized prior to the current development design, and relocating access would require substantial reconfiguration and additional property acquisition, which is not feasible.
3. **Topography and Adjacent Uses** – Site grading and the location of adjacent improvements constrain alternative access points that would meet the code requirements.

Justification – No Injury to Adjacent Properties

Granting this waiver will not be injurious to neighboring properties. The proposed access point will utilize the existing easement and be designed to meet all applicable sight distance, traffic safety, and turning movement standards. Traffic impacts will be minimal, as the development is expected to generate volumes well below the thresholds requiring a full traffic impact analysis under Sec. 107-317(a).

Justification – No Prevention of Orderly Subdivision

This waiver will not hinder the orderly subdivision or development of surrounding land. The access arrangement is contained entirely within existing property boundaries and recorded easements, and will not require or prevent any changes to adjoining parcels’ access rights. Any future subdivision of nearby land can proceed in accordance with the City’s access management requirements without being impacted by this waiver.

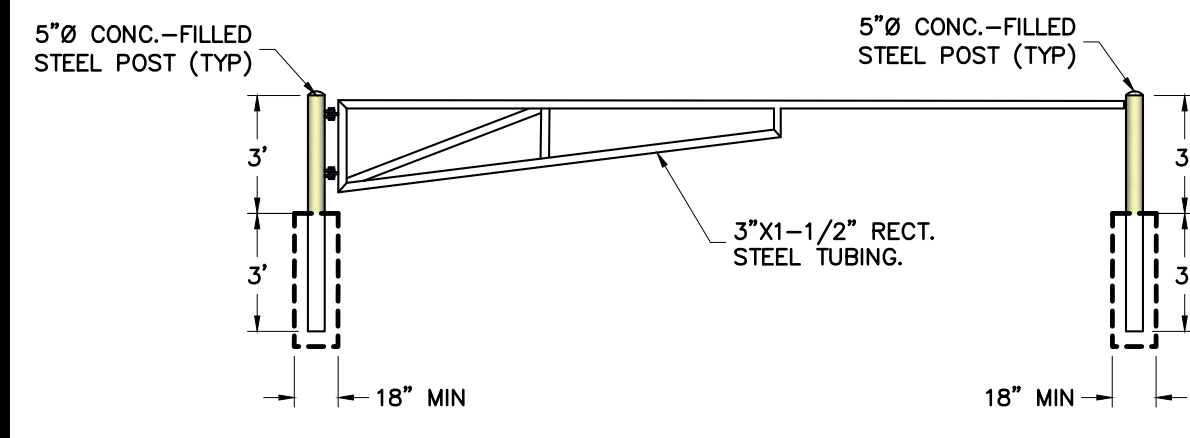
In conclusion, the physical constraints of the property and the preexisting access easement create a unique hardship that makes strict compliance with Sec. 107-317 impractical. The proposed access location is safe, reasonable, and consistent with sound traffic engineering principles, while avoiding any adverse effects on neighboring properties or the broader roadway network.

Thank you for your consideration of this request.

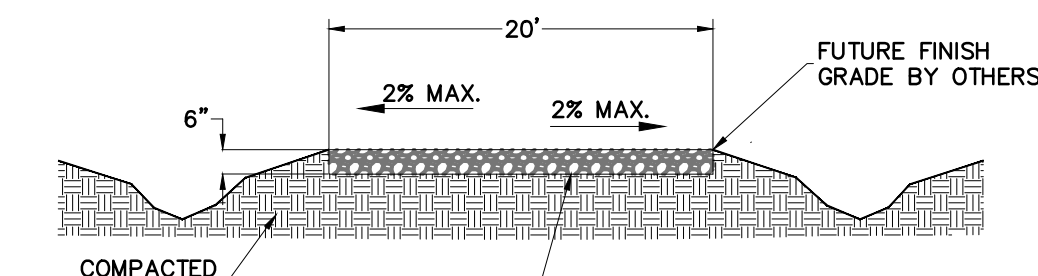
Sincerely,



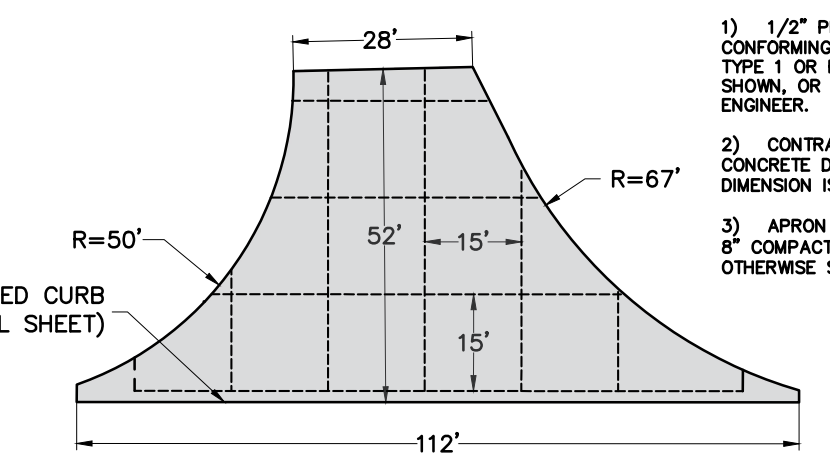
Charlie Holt, P.E.
Director of Water
Bella Vista POA



BARRIER GATE DETAIL

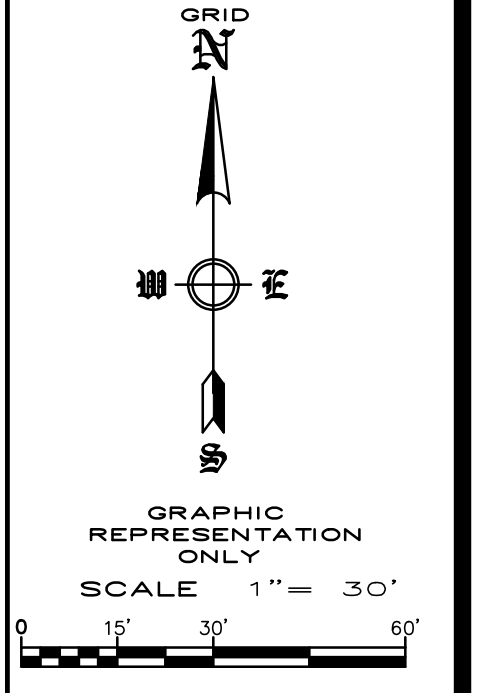


ACCESS ROAD DETAIL

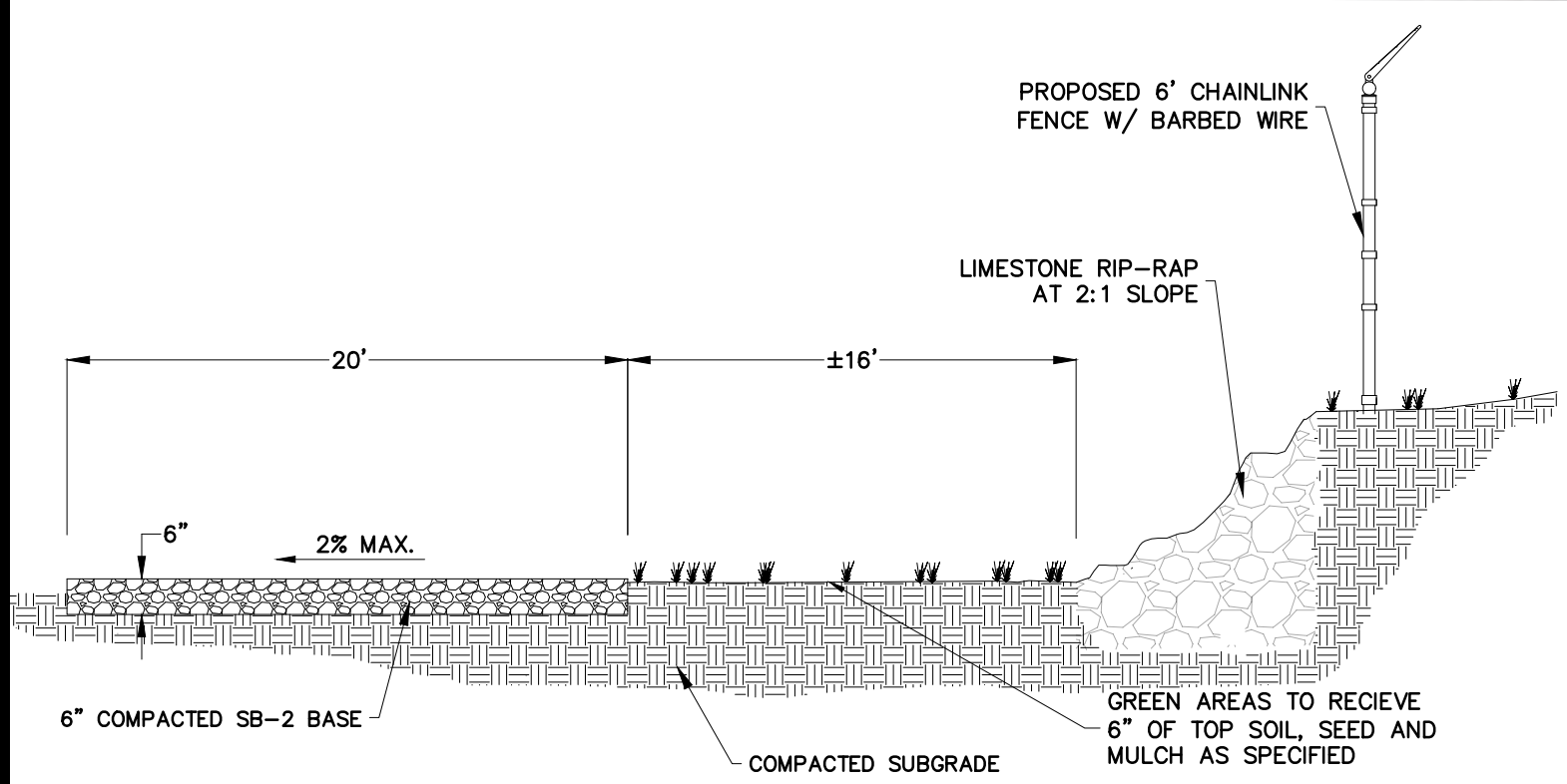
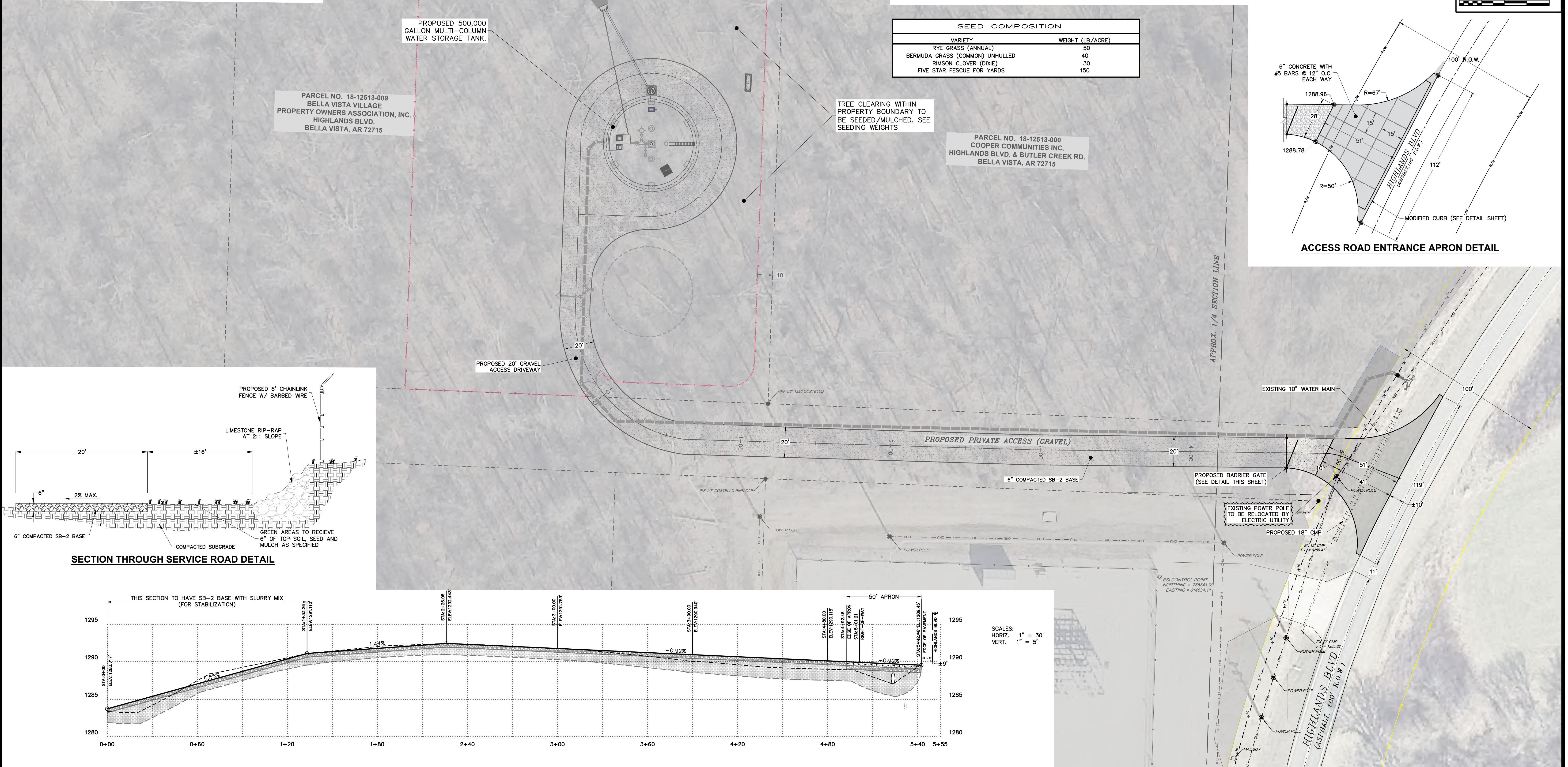


ACCESS ROAD ENTRANCE APRON SAWCUT DETAIL

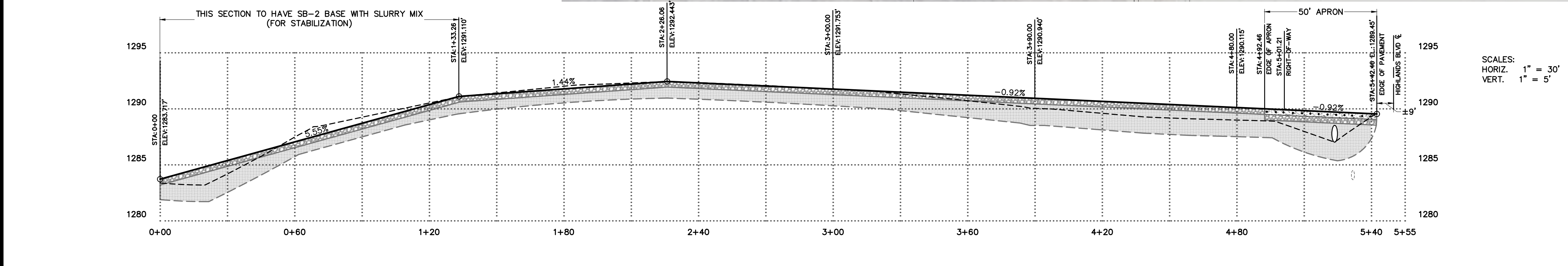
- NOTES:**
- 1/2" PREFORMED EXPANSION MATERIAL CONFORMING TO ASTM A615, ASTM-1752, TYPE 1 OR REDWOOD SHALL BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
 - CONTRACTION JOINTS TO BE PLACED IN CONCRETE DRIVEWAY SO THAT NO SLAB DIMENSION IS MORE THAN 15'.
 - APRON SECTION TO BE 6" CONCRETE WITH 8" COMPACTED SB-2 BASE COURSE UNLESS OTHERWISE SPECIFIED.



SEED COMPOSITION	
VARIETY	WEIGHT (LB/ACRE)
RYE GRASS (ANNUAL)	50
BERMUDA GRASS (COMMON) UNHULLED	40
RIMSON CLOVER (DIXIE)	30
FIVE STAR FESCUE FOR YARDS	150



SECTION THROUGH SERVICE ROAD DETAIL



ACCESS ROAD PROFILE

- GENERAL NOTES:**
- All water mains to have a minimum cover of 36 inches over the top of the pipe.
 - The owner will provide easements where transmission mains cross private property.
 - The contractor shall locate all existing utilities before proceeding with construction. Existing utilities typically are not shown on these plans.

LEGEND

-----	PROPOSED 2" WATER MAIN	□	PROPOSED AIR RELEASE VALVE
-----	PROPOSED 3" WATER MAIN	○	PROPOSED PRESSURE REDUCING VALVE
-----	PROPOSED 4" WATER MAIN	○	PROPOSED GATE VALVE
-----	PROPOSED 6" WATER MAIN	○	PROPOSED BLOW-OFF
-----	PROPOSED 8" WATER MAIN	○	PROPOSED 3-WAY FIRE HYDRANT
-----	PROPOSED 12" WATER MAIN	○	PROPOSED 2-WAY FIRE HYDRANT
-----	EXISTING WATER MAIN & SIZE	○	PROPOSED LEAK DETECTION METER
●	SINGLE METER SETTING	○	
●	DOUBLE METER SETTING	○	
●	SINGLE METER SETTING	○	
●	1" METER SETTING	○	

REVISION	DATE	DESCRIPTION

SCALE: 1" = 30'
 DATE: Aug 27, 2025
 ENGINEER: TAM
 DRAFTER: JAL
 © COPYRIGHT 2025, ENGINEERING SERVICES INC.

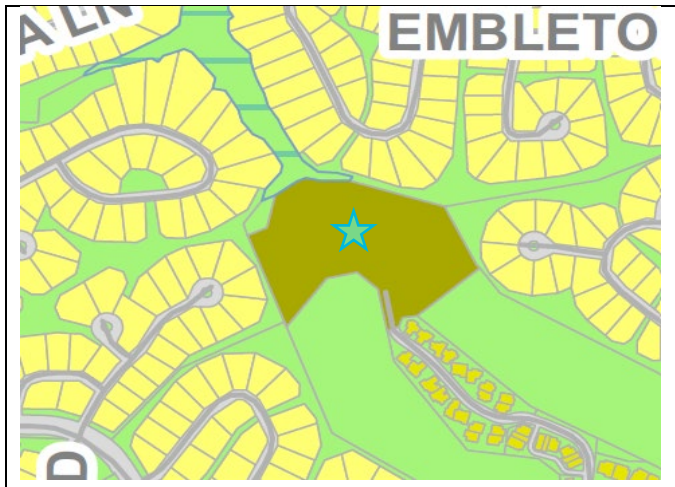
TANK SITE GRADING PLAN
PROPOSED 500,000 GALLON WATER STORAGE TANK
 BELLA VISTA PROPERTY OWNERS ASSOCIATION
 BENTON COUNTY, ARKANSAS

ESI 24828
 SHEET No. 07
 ENGINEERING SERVICES INC.
 1207 SOUTH OLD MISSOURI RD.
 SPRINGDALE, ARKANSAS 72776-4
 PHONE: 479-751-8733

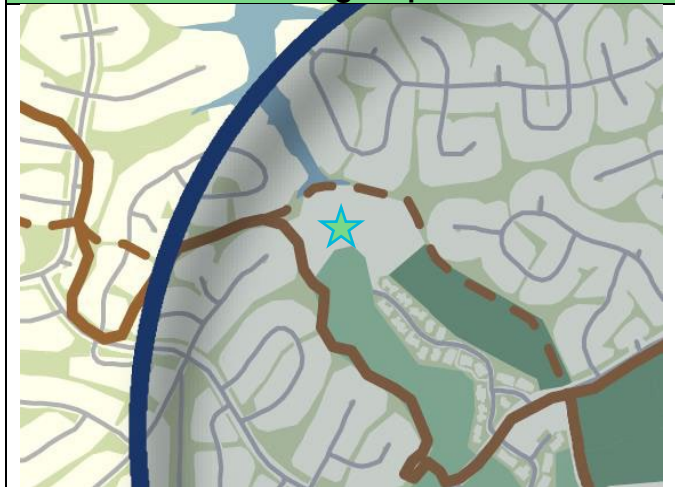


Meeting Information:
 2483 Forest Hill Blvd
 October 2, 2025 at 4:30 pm
Reviewer:
 Christopher Hyatt
 Senior Planner

Project Number	WVR-61092 & PPL-61139
Applicant	Linda Lloyd
Address/Location	102 Fairway Drive
Current Zoning	Planned Zoning District
Site Area	+/- 7.69 acres
Nature of Request	Seeking a waiver of Sec. 107-250(m) along with a preliminary plat request.



Zoning Map



Future Land Use Plan

Property Description

This property is located in the southeast area of Bella Vista City Limits and is located on Parcel #16-11099-000. Lot 1, Block 6 of the Duxford Subdivision.

Regulation

Sec. 107-250(m) regulates maximum sidewalk/trail slope percentages. Sec. 107-71 through 77 establishes preliminary plat approval requirements.

Request

The applicant is proposing a preliminary plat, creating 23 residential lots and seeking a waiver on 107-250(m).

Background

Following the approval of the Planned Zoning District for Owl Commons, this proposal takes the existing singular lot and establishes 23 new lots. The new lots will provide for 23 single-family units complimented by two community building spaces. The proposed plat keeps the existing drainage and utility easements along the outer perimeter of the original property boundary and creates new drainage easements between the outer, residential lots. The PZD established zero lot line setbacks and utilities to be determined by utility companies. This request will establish a new, **private road** (Owl Loop) to be maintained by their HOA that includes 50' of access easement with 24' of paved surface. The proposed 50' easement will also serve as a utility easement. A 5' wide sidewalk will be established along the outer edge of Owl Loop and will transition to an internal layout around the interior community spaces of the development meeting the intent of 107-250(m). The Fire Chief and City Engineer has approved the street grade. Carroll Electric had no additional comments at this time. Water will be served via POA water and Village Wastewater will serve the sewer needs.

Outstanding Technical Comments

A few areas of comments are being confirmed at the time of this report.

Staff Recommendation

None at this time of this report.

Sec. 107-35 Waiver Review Requirements

When, by the strict interpretation of Chapter 107, an applicant incurs undue restrictions on the physical property to be subdivided or developed, a waiver for such requirements may be granted by the planning commission. **Under no circumstance** should a waiver be granted because of a personal hardship or for personal or emotional reasons. Waivers shall not be granted based strictly on financial hardship. A waiver is determined by the strict interpretation and enforcement of the rules and regulations upon a given piece of property to be subdivided or developed. No waiver shall be granted unless the planning commission finds all of the following:

1. That there are special circumstances or conditions affecting the land involved such that the strict application of the provision of these regulations would deprive the applicant of the reasonable use of this land.
2. That the waiver is necessary for the preservation and enjoyment of a substantial property right of the applicant.
3. That the granting of the waiver will not be detrimental to the public health, safety and welfare or injurious to other property in the area.
4. That the granting of the waiver will not have the effect of preventing the orderly subdivision of other land in the area in accord with the provision of these regulations.

Four affirmative votes of the planning commission's authorized membership shall be necessary for the passage of any waiver.

Sec. 107-35 Waiver Review Procedures

- Under exceptional circumstances, the planning commission may grant waivers at the time of final plat approval.
- In granting the waiver, the planning commission shall prescribe any conditions that it deems necessary to or desirable in the public interest.
- In considering the petition for a waiver, the planning commission shall take into account the nature of the proposed use of land involved, existing uses of land in the area, proximity to public utilities, the number of persons who will reside or work in the proposed subdivision, and the probable effect of such waiver upon traffic conditions and upon the public health, safety and general welfare in the vicinity.
- The findings of the planning commission, together with the specific facts upon which findings are based, shall be incorporated into the official minutes of the planning commission meetings at which such waiver is granted. Waivers may be granted only when in harmony with the general purpose and intent of these regulations.

Sec. 107-74,77 Preliminary Plat Review and Approval Procedures

1. After the planning commission has reviewed the preliminary plat and taken into account any staff recommendation, the applicant shall be advised of any required changes and/or additions.
2. The planning commission shall approve, approve conditionally, or disapprove the preliminary plat within 60 days from the date of receipt thereof or the preliminary plat shall be deemed approved unless the subdivider stipulates in writing to the planning commission that additional time is allowed. If disapproved, the preliminary plat shall be returned to the subdivider with a written statement as to the reasons for disapproval.
3. Should the Planning Commission Disapprove a preliminary plat:
 - a. A disapproved preliminary plat may be resubmitted. The plat shall be submitted for review as outlined in this regulation for an original preliminary plat submission.
 - b. The mayor or his designee may forego those steps in the review process of a resubmitted plat found to be entirely repetitive of the disapproved plat.
4. The preliminary plat approval shall automatically expire one year from the original date of approval and further development work will require approval of another preliminary plat. An approved preliminary plat conditioned upon the developer completing a list of deficiencies shall also be considered to be null and void should the list of deficiencies in its entirety not be completed within the designated six-month period from the date of preliminary plat approval.

Sec. 107-75 Preliminary Plat Review Criteria

1. Definite provision has been made for a water supply system that is sufficient in terms of quantity, dependability, fire protection, and quality to provide an appropriate supply of water for the type of subdivision proposed. The applicant shall provide verification of approval from the state department of health or governing utility.
2. If a public sewage system is proposed, adequate provision has been made for such a system and, if other methods of sewage disposal are proposed, that such systems will comply with federal, state and local laws and regulations. The applicant shall provide verification of approval from the state department of health or governing utility.
3. All areas of the proposed subdivision which may involve soil or topographical conditions presenting hazards or requiring special precautions have been identified by the subdivider and that the proposed uses of these areas are compatible with such conditions.
4. Transportation needs are met:
 - a. The existing transportation system is adequate to accommodate the traffic to be generated by the subdivision. The planning commission may require, as part of plat approval, a traffic study, prepared by professional traffic engineer and paid for by the developer, demonstrating that existing streets can handle the proposed traffic. The city also may require that the developer provide plans and profiles showing existing ground surface, proposed and established street grades, including extensions for a reasonable distance beyond the limits of the proposed development.
 - b. If the traffic study indicates that the development will create more demand than the existing transportation system can accommodate, the developer shall show that it will make appropriate off-site improvements to meet the increase in demand and capacity.



OWL COMMONS

nature & community

8/22/2025

Letter to the Bella Vista Planning Commission -
Ref - Waiver application regarding Owl Commons PZD

Section 107-250(m) allows for a waiver of the Pedestrian Accommodation section requiring that sidewalks/trails be less than 8% slope.

The Owl Commons PZD was unanimously approved by the City Council 6/23. We are now submitting the Preliminary Plat request. The engineer worked very hard to detail the interior sidewalks/trails at below 8%, but due to the topography of the site he was unable to do so. His proposal was to then add a variety of sets of stairs on these sidewalks.

Owl Commons is a 55+ co-housing neighborhood based on nature and community. We are requesting this variance to exceed the required 8% slope based on the following -

- topographical difficulties
- tree preservation
- aesthetics as provided in the Code.

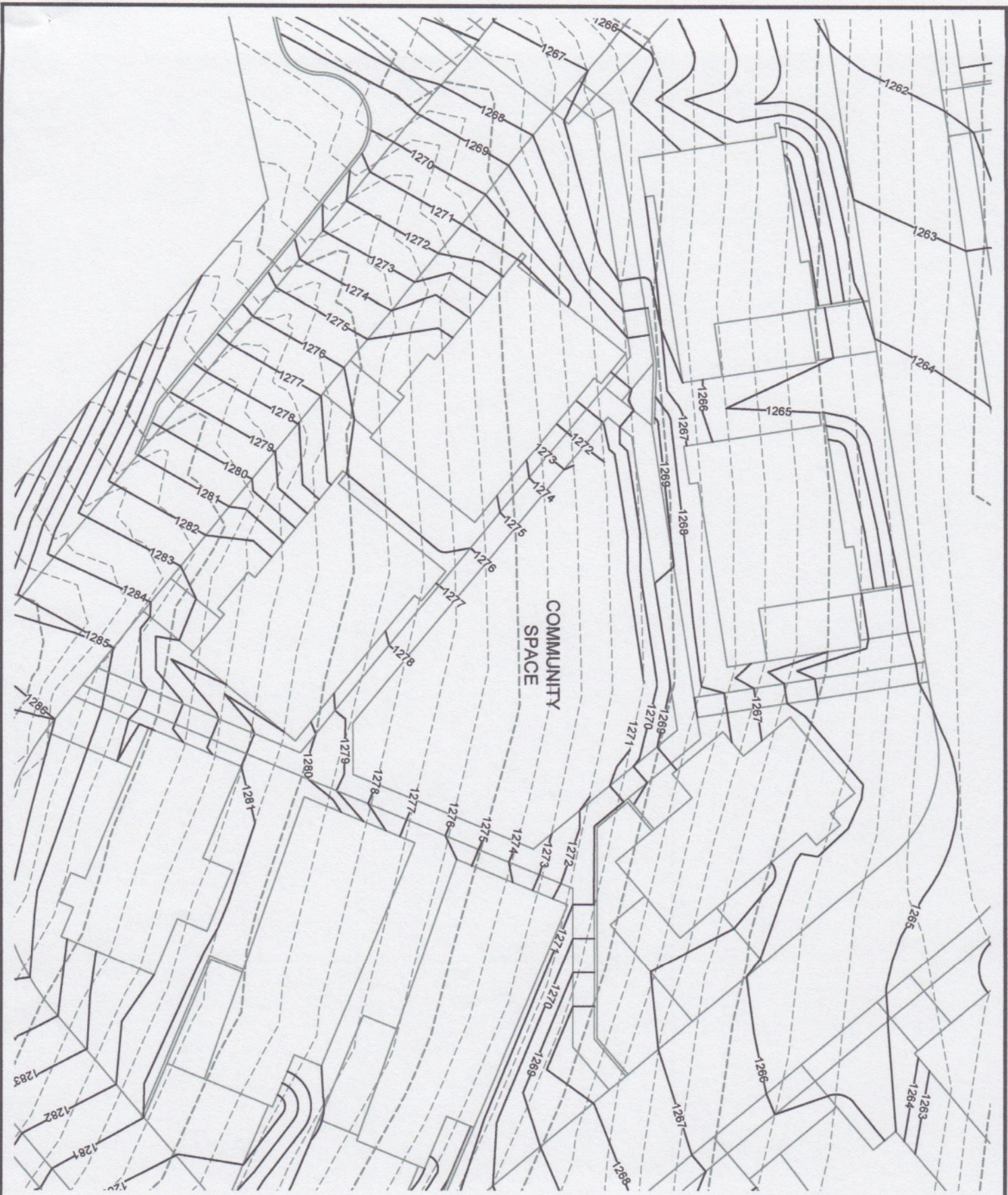
It is important for us to save as many trees on this site as we can as indicated in our PZD application. Using the existing topography as much as possible will accommodate that goal. I also believe that slightly steeper sections (as I believe are on the Branchwood walking trail) would accommodate our 55+ residents more than a series of stepped areas would.

Owl Commons is not an urban San Francisco neighborhood, it is a community set amidst the woods of Bella Vista. Steps would change that aesthetically.

This request would not be injurious to other property in the area or affect orderly subdivision of other lands, as these sidewalk areas are internal and would not be dedicated to the City of Bella Vista.

Thank you for your consideration.

Lucia Loya 8/22/25



COMMUNITY
SPACE



PROJECT NUMBER:
230082
DATE:
08/25/2025

**SIDEWALK SLOPE
VARIANCE INFORMATION
COMMUNITY SPACE 1
OWL COMMONS
102 FAIRWAY DRIVE, BELLA VISTA, AR**

LEGEND

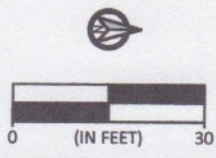
	SPOT ELEVATIONS
	PROPOSED CONTOURS
	EXISTING CONTOURS

**ZANEVAN
ENGINEERING**

PHONE: 417-800-2500
1221 OAK ST.
CARTHAGE, MO 64836



COMMUNITY
SPACE



PROJECT NUMBER:
230082
DATE:
08/25/2025

**SIDEWALK SLOPE
VARIANCE INFORMATION
COMMUNITY SPACE 2
OWL COMMONS
102 FAIRWAY DRIVE, BELLA VISTA, AR**

- LEGEND**
- SPOT ELEVATIONS
 - PROPOSED CONTOURS
 - EXISTING CONTOURS

PHONE: 417-500-2500
1221 OAK ST.
CARTHAGE, MO 64836

From: [Owl Commons](#)
To: [Christopher Hyatt](#)
Cc: [Bella Vista Planning](#)
Subject: POA permissions
Date: Wednesday, September 24, 2025 2:44:30 PM
Attachments: [image003.jpeg](#)
[5.png](#)

[WARNING: External Email] Do not click links or open attachments unless you trust the sender.

From: Doug McCash <doug@bvvpoa.com>
Sent: Wednesday, September 17, 2025 9:12 AM
To: Owl Commons <info@owlcommons.com>; Charlie Holt <charlieh@bvvpoa.com>
Cc: jason <jason@zanevan.com>; Mark Stanley <mark@zanevan.com>
Subject: RE: Owl Commons URGENT

Linda,

Provided there is no damage to the golf course, the POA will allow grading within 5' of the property line. Also, provided there is no additional sediment buildup or runoff into the lake, the POA is agreeable to allowing increased water flow into the lake.

image003.jpeg



Doug McCash JD, CMCA, AMS
Bella Vista POA
General Counsel
(479) 855-5015
doug@bvvpoa.com
bellavistapoa.com

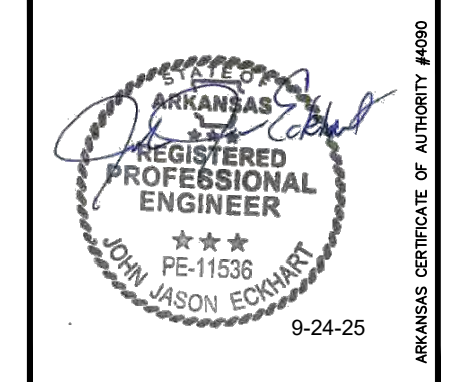




LAND USE CATEGORIES

USE TYPE	ZONING DISTRICT	ACREAGE
 OPEN SPACE / RECREATION	OS/R	4.06 AC
 RESIDENTIAL SINGLE FAMILY	RSF	2.10 AC
 COMMUNITY FACILITY	CF	0.14 AC
LEGEND		
 VEHICLE PEDESTRIAN CIRCULATION		1.39 AC

- PLANNED ZONING DISTRICT (PZD) BUILDING SETBACKS:**
1. MINIMUM FRONT SETBACK FROM ALL PROPERTY LINES SHALL BE 0 FEET
 2. A MINIMUM SETBACK OF 0 FEET IS ESTABLISHED PARALLEL TO ALL STREET RIGHT-OF-WAY LINES
 3. A MINIMUM SIDE SETBACK OF 0 FEET IS ESTABLISHED PARALLEL TO SIDE PROPERTY LINES.
 4. A MINIMUM REAR SETBACK OF 0 FEET IS ESTABLISHED PARALLEL TO SIDE PROPERTY LINES.
 5. ALONG BODIES OF WATER, LOTS OR PARCELS WITH ACCESS TO BODIES OF WATER SHALL HAVE AT LEAST A 25-FOOT SETBACK FROM THE WATER'S EDGE IN WHICH NO PRIMARY OR ACCESSORY STRUCTURES MAY BE BUILT OTHER THAN A KAYAK DOCK AND KAYAK STORAGE RACK.
 6. ALONG GOLF COURSE LOTS OR PARCELS ADJOINING A GOLF COURSE, THE MINIMUM BUILDING SETBACK IS 20 FEET.

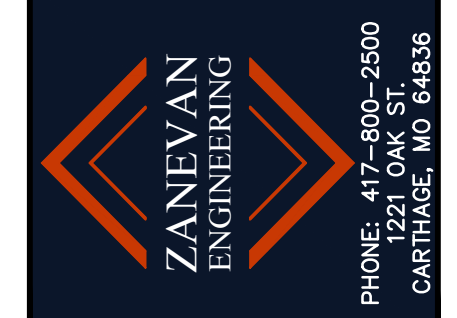


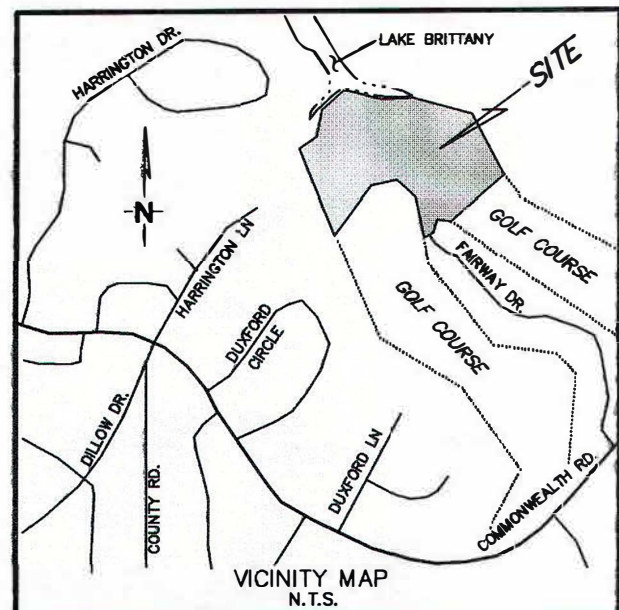
REVISIONS

NO.	DESCRIPTION	BY	DATE

OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-2.0
 ZONING AND
 DEVELOPMENT PLAN
 PROJECT NUMBER:
 230082
 DATE:
 08/28/2025





FLOOD NOTE

THE HEREON DESCRIBED PROPERTY IS LOCATED WITHIN ZONE "X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN), AND ZONE "AE" (FLOOD HAZARD ZONE) AS DETERMINED BY SCALING FROM THE NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP FOR BENTON COUNTY, ARKANSAS, MAP NUMBER: 05007C0080 J. MAP EFFECTIVE DATE: SEPTEMBER 28, 2007.

NOTES:

- ENVIRONMENTAL AND SUBSURFACE CONDITIONS WERE NOT EXAMINED AS A PART OF THIS SURVEY.
- SURVEYOR HAS MADE NO INVESTIGATION OR INDEPENDENT SEARCH FOR EASEMENTS OF RECORD, ENCUMBRANCES, RESTRICTIVE COVENANTS, TITLE EVIDENCE, ZONING RESTRICTIONS, OR ANY OTHER FACTS WHICH AN ACCURATE AND CURRENT TITLE SEARCH MAY DISCLOSE.
- DECLARATION IS MADE TO THE ORIGINAL PURCHASER OF THE SURVEY AND IS NOT TRANSFERABLE.
- BASIS OF BEARINGS: ARKANSAS STATE PLANE GRID, NAD-83, NORTH ZONE, BASED ON GPS OBSERVATION. COORDINATES SHOWN ARE BASED OFF NGS MONUMENT PID-DL5975 HAVE PUBLISHED COORDINATES OF: LAT. 36°23'38.18787, LONG. 094°12'31.92408, CONVERGENCE -117°07.2
- COUNTY JUDGMENT/COURT RECORDS WERE NOT EXAMINED AS A PART OF THIS SURVEY.
- ALL DISTANCES SHOWN HEREON ARE GROUND DISTANCE UNLESS OTHERWISE NOTED.
- THIS PLAT CONTAINS ONE LOT.
- PER THE DUXFORD RECORD PLAT, BLOCKS 1-5, THE PROPERTY LINE ALONG THE LAKE IS A MEANDER LINE AT A CONSTANT ELEVATION ONE FOOT HIGHER THAN THE HIGHEST SPILLWAY ELEVATION OF 1200' M.G.L. ±0.5', AS DETERMINED FROM BENCHMARK 1318, C.W.A. 1934 (U.S.G.S.).
- WATER LINE LOCATIONS SHOWN HEREON ARE PER P.O.A. MARKINGS.

NOTES CONSTITUTING A PART OF THIS PLAT AND TO BE READ IN CONNECTION WITH THE PLAT AND ALL PROPERTY REFLECTED THEREON

COOPER COMMUNITIES, INC. HEREINAFTER REFERRED TO AS DEVELOPER (FORMERLY NAMED COOPER ACQUISITION CORPORATION AND SUCCESSOR IN INTEREST TO THE PREDECESSOR COOPER COMMUNITIES INC. AND JOHN A. COOPER COMPANY BY REASON OF MERGERS, THE SAID JOHN A. COOPER COMPANY FORMERLY BEING NAMED CHEROKEE VILLAGE DEVELOPMENT COMPANY, INC.) IS THE OWNER OF ALL REAL ESTATE REFLECTED BY THIS PLAT, AND THE PLAT IS FILED FOR RECORD AND RECORDED SUBJECT TO THE FOLLOWING PROVISIONS:

- AT 10:00 O'CLOCK A.M. ON THE 18TH DAY OF MAY, 1965, THE DEVELOPER, JOINED BY BELLA VISTA COUNTRY CLUB (NOW BELLA VISTA VILLAGE PROPERTY OWNERS ASSOCIATION) A NON-PROFIT CORPORATION, FILED IN THE OFFICE OF THE CIRCUIT CLERK AND EX-OFFICIO RECORDER IN AND FOR BENTON COUNTY, ARKANSAS, A DECLARATION WHICH DECLARATION IS THERE RECORDED IN BOOK 373, PAGE 08, ET. SEQ.
- THIS PLAT IS FILED CONTEMPORANEOUSLY WITH THE FILING OF THE SUPPLEMENTAL DECLARATION OF COVENANTS AND RESTRICTIONS, EXECUTED BY THE DEVELOPER WHICH HAS THE EFFECT OF BRINGING THE LANDS REFLECTED UPON THE PLAT WITHIN THE PROVISIONS OF THE DECLARATION AFORESAID FILED FOR RECORD ON MAY 18, 1965, AFORESAID, AND LIKEWISE THE DECLARATION FILED FOR RECORD ON MAY 18, 1965 AFORESAID, IN ITS ENTIRETY IS BY REFERENCE MADE A PART OF THIS PLAT; THE PROVISIONS OF THE DECLARATION AFORESAID SHALL CONTROL AS TO THE PLAT EXCEPT ONLY AS TO THE PROVISIONS HEREIN CONTAINED.
- THE COMMON PROPERTIES REFLECTED UPON THE PLAT ARE INTENDED TO BE DEVOTED TO THE COMMON USE AND ENJOYMENT OF THE OWNERS OF THE LOTS REFLECTED UPON SAID PLAT AS WELL AS THE OWNERS OF ALL OF THE PROPERTIES AS SO DEFINED AND IN THE DECLARATION AFORESAID AND IN NOWISE SHALL BE CONSIDERED AS DEDICATED TO THE GENERAL PUBLIC.
- UNLESS SHOWN OTHERWISE ON THE PLAT, ALL LOTS HAVE A SEVEN AND ONE-HALF FOOT UTILITY AND DRAINAGE EASEMENT (RESERVED BY THE DEVELOPER) ON THE INTERIOR OF ALL LOT LINES, SUCH EASEMENTS TO BE PARALLEL WITH THE CORRESPONDING LOT LINE.
- NO SINGLE FAMILY DETACHED STRUCTURE SHALL BE CONSTRUCTED WHICH SHALL HAVE A FLOOR SPACE OF LESS THAN 1200 SQUARE FEET.
- THE CART PATH AND THE UTILITY AND INGRESS/EGRESS EASEMENT IS HEREBY DEDICATED TO THE BELLA VISTA PROPERTY OWNERS ASSOCIATION AS SPECIFICALLY DESIGNATED ON THE PLAT.
- SETBACK LINES AS PRESCRIBED BY THE ARCHITECTURAL CONTROL COMMITTEE AND THE CITY OF BELLA VISTA SHALL CONTROL AS TO CONSTRUCTION OF ANY STRUCTURES UPON THE PROPERTIES REFLECTED THEREON, SUBJECT, HOWEVER, TO THE PROVISIONS OF THE DECLARATION AFORESAID.
- DEVELOPER, ITS SUCCESSORS AND ASSIGNS, HEREBY RESERVE A PERPETUAL BLANKET EASEMENT, PRIVILEGE AND RIGHT IN, UPON, OVER AND ACROSS THE COMMON PROPERTIES REFLECTED HEREON, IF ANY, FOR PURPOSES OF CONSTRUCTING AND MAINTAINING SUCH ROADS, STREETS OR HIGHWAYS AS IT SHALL DETERMINE TO BE NECESSARY OR DESIRABLE IN ITS SOLE DISCRETION, INCLUDING SUCH CUTS, GRADING, LEVELING, FILLING, DRAINING, PAVING, BRIDGES, CULVERTS, RAMPS AND ANY OR ALL OTHER ACTIONS OR INSTALLATIONS WHICH IT DEEMS NECESSARY OR DESIRABLE FOR SUCH ROADS, STREETS OR HIGHWAYS TO BE SUFFICIENT FOR ALL PURPOSES OF TRANSPORTATION AND TRAVEL. THE WIDTH AND LOCATIONS OF THE RIGHT OF WAY FOR SUCH ROADS, STREETS OR HIGHWAYS SHALL BE WITHIN THE SOLE DISCRETION OF DEVELOPER, ITS SUCCESSORS AND ASSIGNS, PROVIDED, HOWEVER, THAT DEVELOPER, ITS SUCCESSORS AND ASSIGNS, WILL USE ITS BEST EFFORTS CONSISTENT WITH ITS PURPOSES TO LESSEN ANY DAMAGE OR INCONVENIENCE TO IMPROVEMENTS WHICH HAVE THEREFORE BEEN LOCATED UPON THE PROPERTY. DEVELOPER, ITS SUCCESSORS AND ASSIGNS, FURTHER RESERVES THE UNRESTRICTED AND SOLE RIGHT AND POWER OF DESIGNATING SUCH ROADS, STREETS OR HIGHWAYS AS PUBLIC OR PRIVATE AND OF ALIENATING AND RELEASING THE PRIVILEGES, EASEMENTS AND RIGHTS RESERVED HEREIN.

DATED THIS 13th DAY OF June 2013.

WE THE UNDERSIGNED, OWNER OF THE REAL ESTATE SHOWN AND DESCRIBED HEREIN, DO HEREBY CERTIFY THAT WE HAVE LAID OFF, PLATTED, AND SUBDIVIDE AND DO HEREBY LAY OFF PLAT, SUBDIVIDE SAID REAL ESTATE IN ACCORDANCE WITH THIS PLAT.

DATE OF EXECUTION: June 13th, 2013

COOPER COMMUNITIES, INC., OWNER

BY: *[Signature]*
S. Vice PRESIDENT

COOPER COMMUNITIES, INC.
903 No. 47th Street
Rogers, AR 72756

Source of Title:

CERTIFICATE OF APPROVAL:

PURSUANT TO THE CITY OF BELLA VISTA SUBDIVISION REGULATIONS AND ALL OTHER CONDITIONS AND APPROVALS HAVING BEEN COMPLETED, THIS DOCUMENT IS HEREBY ACCEPTED. THIS CERTIFICATE IS HEREBY EXECUTED UNDER THE AUTHORITY OF THE SAID RULES AND REGULATIONS.

DATE OF EXECUTION:

SIGNED:

BY: *[Signature]*
COOPER COMMUNITIES, INC., DEVELOPER

BY: _____
DIRECTOR OF PLANNING AND CODE ENFORCEMENT

Book 2013 Page 356
Recorded in the Above
PLAT Book & Page
06/25/2013

Brenda DeShields-Circuit Clerk
Benton County, AR
Book/Pg: 2013/356
Term/Cashier: CASH5/Jimmy Bennett
06/25/2013 2:12PM
Tran: 245353
Total Fees: \$15.00

DESCRIPTION:

A PART OF THE RESERVED PROPERTIES OF DUXFORD SUBDIVISION, BLOCKS 1-5, BEING A PART OF THE SE 1/4 OF THE SW 1/4 AND PART OF THE SW 1/4 OF THE SW 1/4 OF SECTION 29, TOWNSHIP 21 NORTH, RANGE 30 WEST, OF THE FIFTH PRINCIPAL MERIDIAN, BENTON COUNTY, ARKANSAS. MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE SW CORNER OF SAID SECTION 29; THENCE N89°58'28"E 981.21 FEET; THENCE N00°01'32"W 489.49 FEET TO THE POINT OF BEGINNING; THENCE N21°49'37"W 30.00 FEET; THENCE N171°32'27"W 379.66 FEET; THENCE N73°55'46"E 55.42 FEET; THENCE N13°47'37"E 150.36 FEET; THENCE N40°27'20"E 121.85 FEET; THENCE S49°22'09"E 37.39 FEET; THENCE S78°59'00"E 60.15 FEET; THENCE S89°44'04"E 109.00 FEET; THENCE S73°56'09"E 361.75 FEET; THENCE S27°30'00"E 281.78 FEET; THENCE S39°49'52"E 20.00 FEET; THENCE S50°04'38"W 303.45 FEET; THENCE S82°07'16"W 58.55 FEET; THENCE S18°56'46"W 42.44 FEET; THENCE S71°04'12"W 46.07 FEET; THENCE N112°08'17"W 176.48 FEET; THENCE N48°50'23"W 106.30 FEET; THENCE S77°42'35"W 117.69 FEET; THENCE S37°09'13"W 254.60 FEET TO THE POINT OF BEGINNING, CONTAINING 7.69 ACRES, MORE OR LESS. SUBJECT TO ANY EASEMENTS, RIGHTS-OF-WAY OR COVENANTS OF RECORD OR FACT.

CERTIFICATE OF APPROVAL:

PURSUANT TO THE CITY OF BELLA VISTA SUBDIVISION REGULATIONS AND ALL OTHER CONDITIONS AND APPROVALS HAVING BEEN COMPLETED, THIS DOCUMENT IS HEREBY ACCEPTED. THIS CERTIFICATE IS HEREBY EXECUTED UNDER THE AUTHORITY OF THE SAID RULES AND REGULATIONS.

DATE OF EXECUTION: 6/13/13

SIGNED:

BY: *[Signature]*
DIRECTOR OF PLANNING AND CODE ENFORCEMENT

ARCHITECTURAL CONTROL COMMITTEE APPROVAL:

THE BELLA VISTA VILLAGE ARCHITECTURAL CONTROL COMMITTEE HEREBY APPROVES THE PLAT OF THE ABOVE MENTIONED LOT AS SHOWN HEREON.

DATED THIS 13 DAY OF June 2013.

BY: *[Signature]*

CHAIRMAN

I, Thomas Costello, hereby certify that this plat correctly represents a boundary survey made by me on May 2012 and boundary markers and lot corners shown hereon actually exist and their location, type and material are correctly shown and all minimum requirements of the Arkansas Minimum Standards for Land Surveyors have been met.

Date: 06/12/2013
Registered Land Surveyor No. 1286



OWNER/DEVELOPER:
COOPER COMMUNITIES, INC.
903 NORTH 47TH STREET
ROGERS, AR 72758
479-246-6500

CLIENT/PURCHASER:
SHELTER CONCEPTS LLC
432 TOWN CENTER NE
BELLA VISTA, AR 72714
479-855-2280

LEGEND

- FOUND MONUMENT AS NOTED
- SET 1/2" REBAR W/CAP OR AS NOTED
- PROPERTY LINE
- - - 20' UTILITY & DRAINAGE EASEMENT
- - - 7.5' UTILITY & DRAINAGE EASEMENT
- - - EASEMENT LINE (AS NOTED)
- - - BUILDING SETBACK LINE
- (NXX°XX'XX"E) RECORD BEARING
- UTILITY POLE W/GUY ANCHOR
- GRATED INLET
- ☆ STREET LIGHT
- ⊕ WATER VALVE
- ⊞ ELECTRIC TRANSFORMER
- OH OVERHEAD ELECTRIC LINE
- RIGHT-OF-WAY LINE
- W WATER LINE

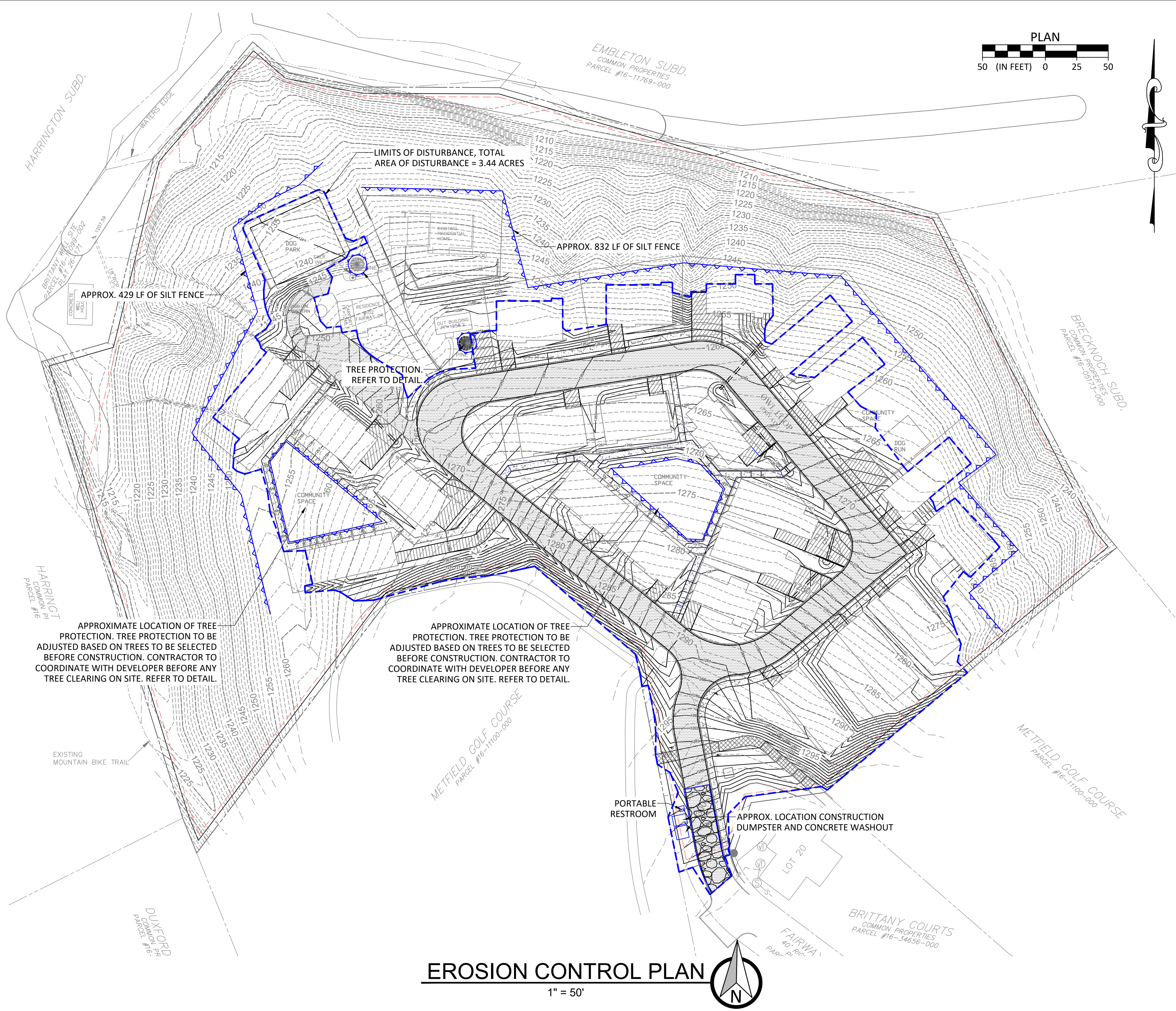
AREA BY QUARTER SECTION
SW 1/4, SW 1/4
4.58 ACRES±
SE 1/4, SW 1/4
3.11 ACRES±

COMPUTED
SW CORNER
SEC. 29-21N-30W
PER PLAT 2-150

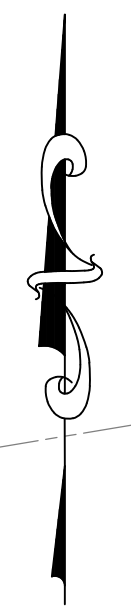
N89°58'28"E
981.21'



GRAPHIC SCALE
(IN FEET)
1 inch = 80 ft.



EROSION CONTROL PLAN
1" = 50'

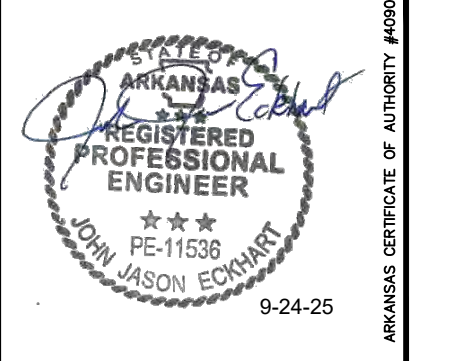


SEDIMENT & EROSION CONTROL GENERAL NOTES

1. THIS PLAN SHOWS THE LOCATION AND DETAILS FOR PRIMARY SEDIMENT CONTROLS TO BE CONSTRUCTED. THE CONTRACTOR IS RESPONSIBLE FOR CONTROLLING EROSION AND DISCHARGE OF SEDIMENT FROM THE SITE AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE NECESSARY MEASURES DURING ALL PHASES OF HIS OPERATIONS REGARDLESS OF WHETHER THEY ARE SPECIFICALLY NOTED ON THIS PLAN AND SHALL MAINTAIN AND REPLACE CONTROLS AS NECESSARY DURING THE COURSE OF HIS OPERATIONS.
2. TEMPORARY CONSTRUCTION ENTRANCE(S) AND SILT FENCES, STRAW BALE DIKES OR OTHER INITIAL SEDIMENT CONTROLS SHOWN ON THIS PLAN MUST BE INSTALLED PRIOR TO ANY OTHER WORK.
3. SEDIMENT BASINS SHOWN ON THIS PLAN MUST BE INSTALLED WITHIN TEN (10) CALENDAR DAYS AFTER CONSTRUCTION BEGINS.
4. THE CONTRACTOR SHALL CLEAN STREETS BOTH INTERIOR AND ADJACENT TO THE SITE, AS NEEDED, AFTER EACH RAINFALL, AND AT THE END OF CONSTRUCTION.
5. THE CONTRACTOR IS RESPONSIBLE FOR CONTROLLING DUST DURING CONSTRUCTION AND SHALL WATER THE CONSTRUCTION AREAS WHENEVER CONDITIONS WARRANT.
6. THE CONTRACTOR IS RESPONSIBLE FOR CLEANING ACCUMULATED SEDIMENT FROM STORM DRAINS PRIOR TO APPROVAL OF CONSTRUCTION.
7. ALL DISTURBED AREAS NOT RECEIVING OTHER PERMANENT STABILIZATION SUCH AS PAVEMENT, ROOFS, SOD, ETC. SHALL BE SEEDED AND MULCHED, AS SPECIFIED BEFORE TEMPORARY SEDIMENT CONTROLS CAN BE REMOVED AND PRIOR TO FINAL APPROVAL OF CONSTRUCTION.
8. THE CONTRACTOR IS RESPONSIBLE FOR INSPECTING BEST MANAGEMENT PRACTICES, (BMP'S), WEEKLY AND AFTER IT RAINS IN ACCORDANCE WITH THE MODNR EROSION CONTROL PERMIT.
9. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING COPIES OF THE SEDIMENT AND EROSION CONTROL PLAN, THE STORMWATER POLLUTION PREVENTION PLAN, (SWPPP), AND THE WEEKLY INSPECTION REPORTS ON SITE AT ALL TIME.
10. APPROVAL FOR THESE EROSION CONTROL PLANS IN NO WAY RELEASES THE DEVELOPER/CONTRACTOR FROM THE RESPONSIBILITY FOR ALL SEDIMENT/EROSION CONTROL METHODS TO BE IN COMPLIANCE WITH THE STANDARDS AND CODES OF THE CITY. THIS INCLUDES, BUT IS NOT LIMITED TO, PROPER MAINTENANCE OF ALL CONTROLS IN PLACE AND PROVIDING ADDITIONAL CONTROL AS REQUIRED FOR BOTH SHEET FLOW AND CONCENTRATED FLOW AREAS.

EROSION CONTROL & MAINTENANCE PLAN NOTES:

1. INSTALLATION OF ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BY THE CITY OR STATE. CONTRACTOR TO VERIFY REQUIREMENTS PRIOR TO BEGINNING ANY WORK ON PROJECT SITE.
2. PROVIDE TEMPORARY EROSION CONTROL TO CONTAIN ALL SOILS ON SITE. COMPLY WITH GOVERNING REGULATIONS PERTAINING TO ENVIRONMENTAL PROTECTION.
3. CONTRACTOR TO RETAIN FLOATABLE WIND BLOWN MATERIALS ON SITE BY STORING ALL TRASH AND BUILDING MATERIAL WASTE IN ENCLOSURES UNTIL PROPER DISPOSAL AT OFF-SITE FACILITIES. CHECK ADJACENT AREAS DAILY AND PICK UP CONSTRUCTION WASTE MATERIALS AND DEBRIS THAT HAVE BLOWN OR WASHED OFF SITE.
4. PERMANENTLY STABILIZE ALL SURFACE AREA WITHIN AND ADJACENT TO THIS SITE THAT IS DISTURBED BY VEHICLES, GRADING AND OTHER CONSTRUCTION FOR THE PROPOSED FACILITY. STABILIZATION IS OBTAINED WHEN THE DISTURBED SURFACE IS COVERED WITH STRUCTURES, PAVING AND OR PERENNIAL VEGETATION HAVING A UNIFORM COVERAGE DENSITY OF AT LEAST 70%. STABILIZATION OF ALL DISTURBED AREA IS REQUIRED BEFORE TERMINATING MAINTENANCE AND REMOVAL OF EROSION CONTROL MEASURES.
5. CONTRACTORS SHALL INSPECT POLLUTION CONTROL MEASURES AT LEAST ONCE EVERY 7 DAYS AND WITHIN 24 HOURS AFTER A STORM EVENT OF 1/2 INCH OR GREATER. DAMAGED MEASURES THAT PROVE TO BE INEFFECTIVE SHALL BE REPLACED WITH MORE EFFECTIVE MEASURES OR ADDITIONAL MEASURES WITHIN SEVEN DAYS. REPEATED FAILURE OF A CONTROL MEASURE REQUIRES INSTALLATION OF A MORE SUITABLE DEVICE TO PREVENT DISCHARGE OF POLLUTANTS FROM THE CONSTRUCTION SITE.
6. CARE SHALL BE TAKEN TO ELIMINATE TO THE MAXIMUM EXTENT POSSIBLE THE ENCROACHMENT OF SEDIMENT INTO ALL STORM DRAIN APPURTENANCES, PUBLIC STREETS, AND ONTO PRIVATE PROPERTY UNTIL IMPERVIOUS MATERIAL (ROAD/PARKING AREA SURFACE) IS APPLIED OR UNTIL PROPOSED LANDSCAPE HAS BEEN ESTABLISHED.
7. REMOVE SEDIMENT DEPOSITS AS NECESSARY AFTER EACH STORM TO PROVIDE ADEQUATE STORAGE VOLUME FOR THE NEXT RAIN AND TO REDUCE PRESSURE ON THE FENCE. CARE NEEDS TO BE TAKEN TO AVOID UNDERMINING THE FENCE WHEN REMOVING SEDIMENT. SEDIMENT IS TO BE REAPPLIED TO THE SITE AND STABILIZED.
8. ALL GRASS SLOPES WHICH EXCEED 3:1 (H:V) AND SELECT PIPE OUTFALLS SHALL UTILIZE TURF REINFORCEMENT. MATS SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATIONS AND STANDARDS. CONTRACTOR SHALL COORDINATE INSTALLATION INSPECTION WITH MANUFACTURER.
9. ALL DISTURBED AREAS ARE TO BE RE-SEEDED IN ACCORDANCE WITH THE CURRENT REQUIREMENTS AND DESIGN STANDARDS.
10. THE DETENTION BASIN, ALL WATER QUALITY MEASURES AND STORMWATER CHANNELS (PIPES) SHALL BE FUNCTIONING PRIOR TO STARTING ANY OTHER CONSTRUCTION ACTIVITIES.
11. CONSTRUCTION ACCESS TO THE SITE SHALL BE LIMITED TO THE APPROVED TEMPORARY CONSTRUCTION ENTRANCE AS SHOWN ON THE STORMWATER POLLUTION PREVENTION PLAN.
12. EROSION CONTROL DEVICES SHALL BE MAINTAINED DURING THE WHOLE CONSTRUCTION PERIOD BY THE CONTRACTOR.
13. THE INSTALLATION OF SILT FENCE FOR CONSTRUCTION IS TO BE INSTALLED BY THE CONTRACTOR AND IN PLACE BEFORE BEGINNING SITE CONSTRUCTION. HAY BALES OR SIMILAR DEVICES ARE TO BE USED BY THE CONTRACTOR TO MEET THE REQUIREMENTS OF THE ENGINEER. DEVICES TO BE IN PLACE AS SHOWN ON THE PLANS. ADJUSTMENT OF THE LOCATION BY THE CONTRACTOR MAY BE DONE TO MEET EXISTING FIELD CONDITIONS. ALL CONTROLS ARE TO BE LACED WITHIN OWNER'S PROPERTY. ACCUMULATED SEDIMENT IN BASINS WILL REQUIRE REMOVAL DURING CONSTRUCTION OR AFTER EACH RAIN EVENT AND AT THE END OF CONSTRUCTION. EACH BASIN SHALL BE CHECKED AFTER EACH RAIN EVENT. CONTRACTOR TO MINIMIZE THE AREA DISTURBED BY CONSTRUCTION ACTIVITIES AT ANY ONE TIME AND TO PROMPTLY RE-VEGETATE (OR MECHANICALLY STABILIZE) ARE DISTURBED BY CONSTRUCTION ACTIVITY.
14. SILT FENCE SHALL BE PLACED AROUND ALL SOIL SPOIL PILES TO PREVENT EROSION.
15. ALL SPOIL PILES SHALL BE REMOVED PRIOR TO COMPLETION OF PROJECT.
16. CONCRETE WASHOUT IS NOT ALLOWED ON SITE. CONCRETE SUPPLIER SHALL PROVIDE OFF-SITE WASHOUT LOCATION.



NO.	DESCRIPTION	DATE	BY

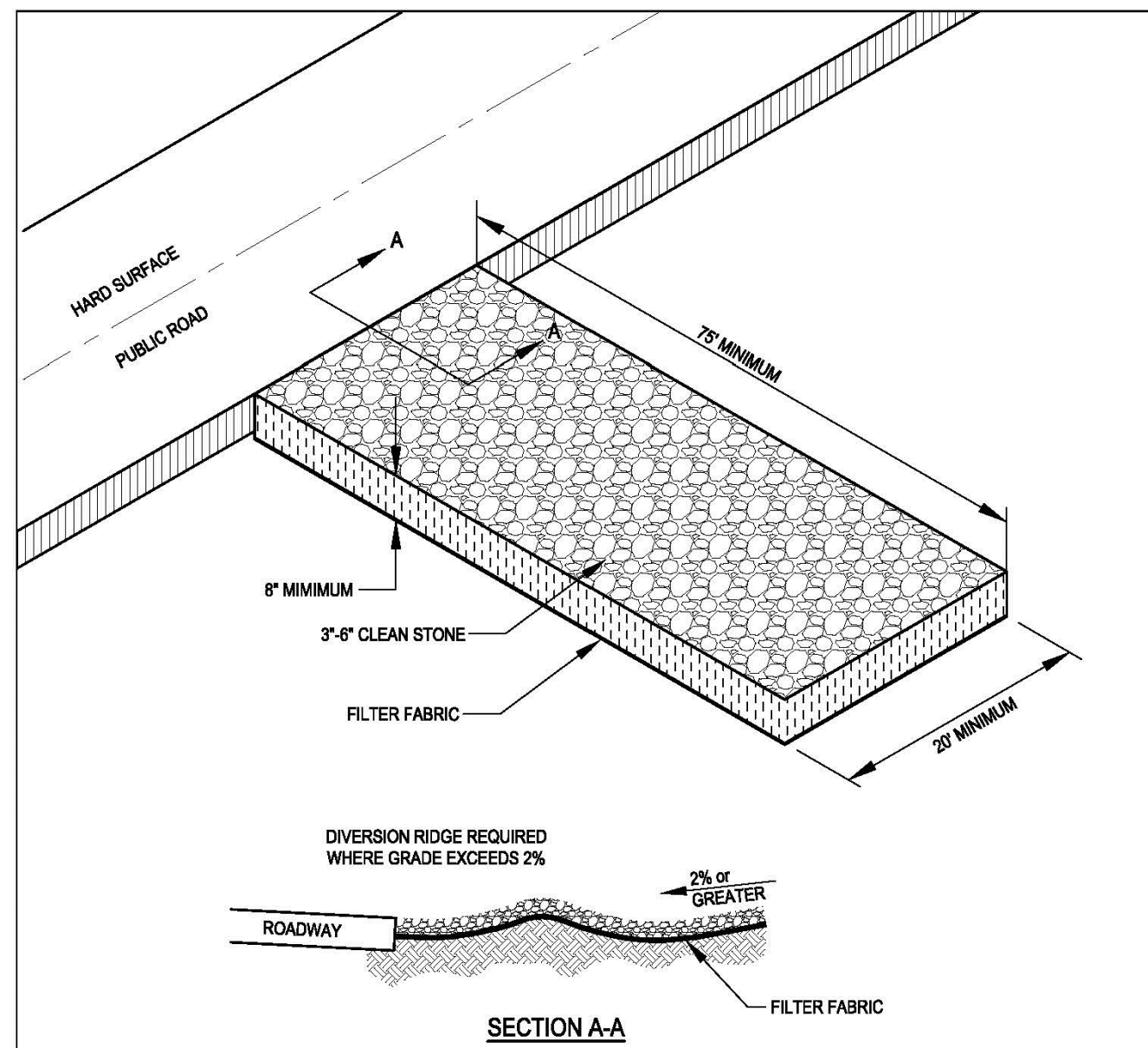
OWL COMMONS

102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-3.0
EROSION CONTROL
PLAN

PROJECT NUMBER:
230082
DATE:
08/28/2025

PHONE: 417-800-2500
1221 OAK ST.
CARTHAGE, MO 64836

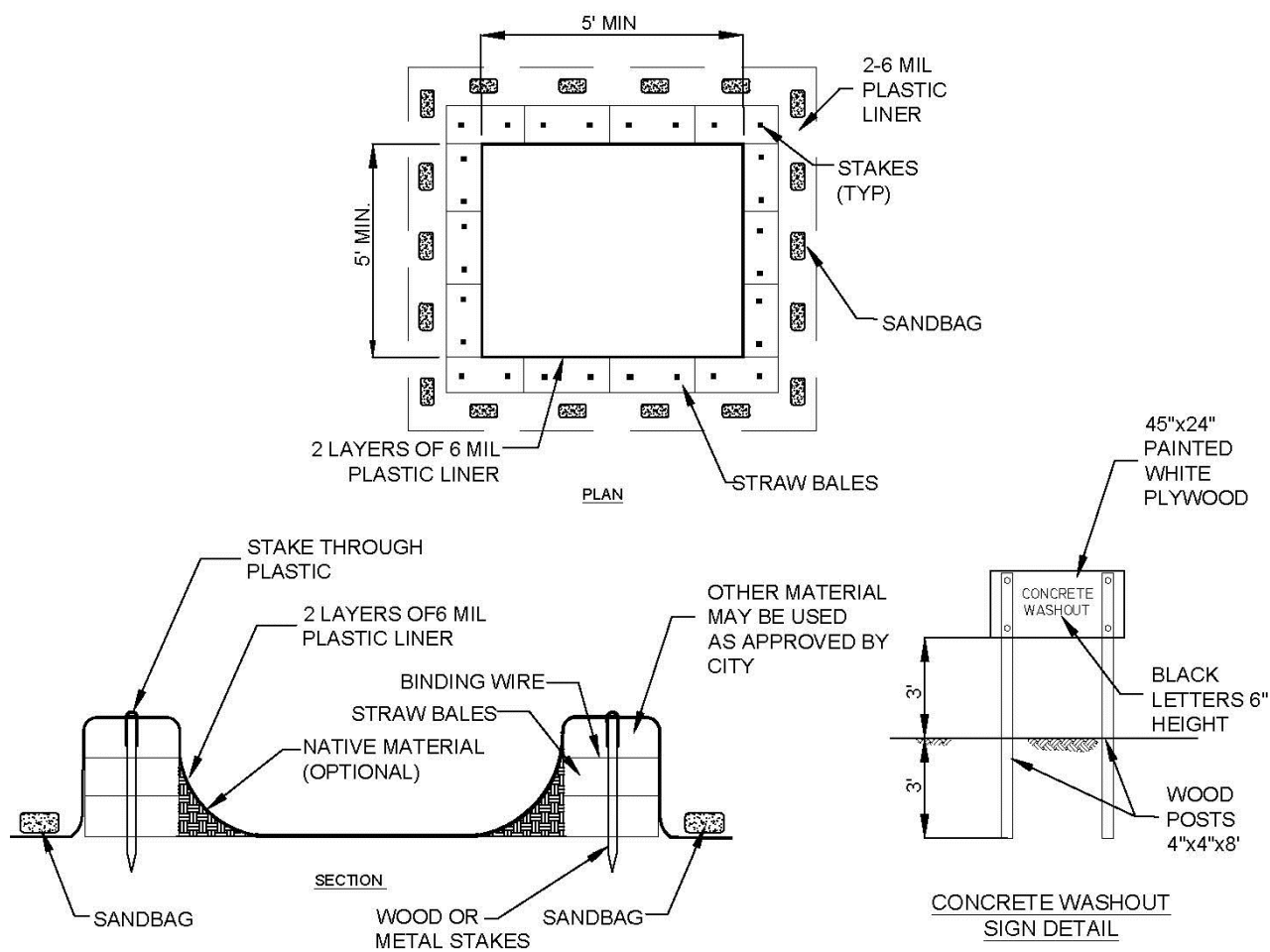


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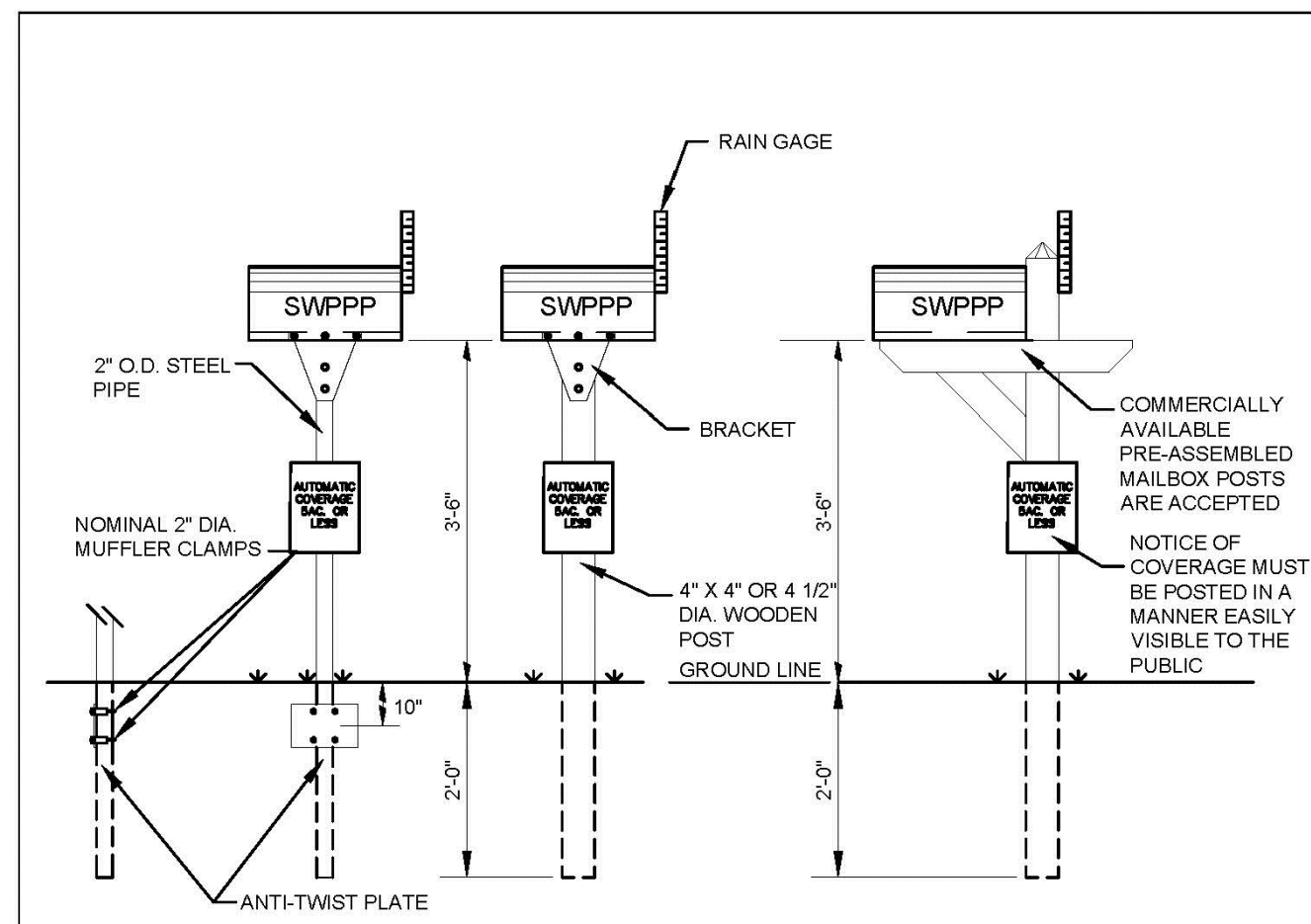
1. REPLACE CONTAMINATED STONE AS REQUIRED TO PREVENT TRACKING OF SEDIMENT OR MUD ON PUBLIC STREETS.
2. CLEAN STREETS DAILY WITH BROOM (NO POWER BROOM) AND SHOVEL. THE USE OF WATER IS PROHIBITED.
3. ALL VEHICLES MUST USE CONSTRUCTION EXIT.
4. IF WHEEL WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.

ENGINEERING DIVISION
EROSION CONTROL DETAILS
 113 W. MOUNTAIN STREET
 CONSTRUCTION EXIT (N.T.S.)
EC1

- NOTES:**
1. NO WASHING OUT OF CONCRETE TRUCKS OR WASHING OF SWEEPINGS FROM EXPOSED AGGREGATE CONCRETE INTO STORM DRAINS, OPEN DITCHES, STREETS, OR STREAMS IS ALLOWED. EXCESS CONCRETE IS NOT ALLOWED TO BE DUMPED ON-SITE, EXCEPT IN DESIGNATED TEMPORARY CONCRETE WASHOUT PIT AREAS.
 2. ON-SITE TEMPORARY CONCRETE WASHOUT AREAS SHALL BE LOCATED AT LEAST 50 FEET FROM STORM DRAINS, OPEN DITCHES, OR WATER BODIES.
 3. TEMPORARY CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED AND MAINTAINED IN SUFFICIENT QUANTITY AND SIZE TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS.
 4. WASHOUT FACILITIES SHALL BE CLEANED OUT OR REPLACED ONCE THE WASHOUT IS 75% FULL. PLASTIC LINING MATERIAL SHALL BE MINIMUM OF 10 MIL POLYETHYLENE SHEETING AND SHALL BE FREE OF HOLES, TEARS, OR OTHER DEFECTS.
 5. WHEN WASHOUT FACILITIES ARE NO LONGER REQUIRED FOR WORK, THE HARDENED CONCRETE SHALL BE REMOVED AND DISPOSED OFF-SITE. MATERIALS USED TO CONSTRUCT TEMPORARY CONCRETE WASHOUT FACILITIES WILL BE REMOVED FROM THE SITE AND DISPOSED OF PROPERLY (i.e. PERMITTED FACILITY OR RECYCLED).



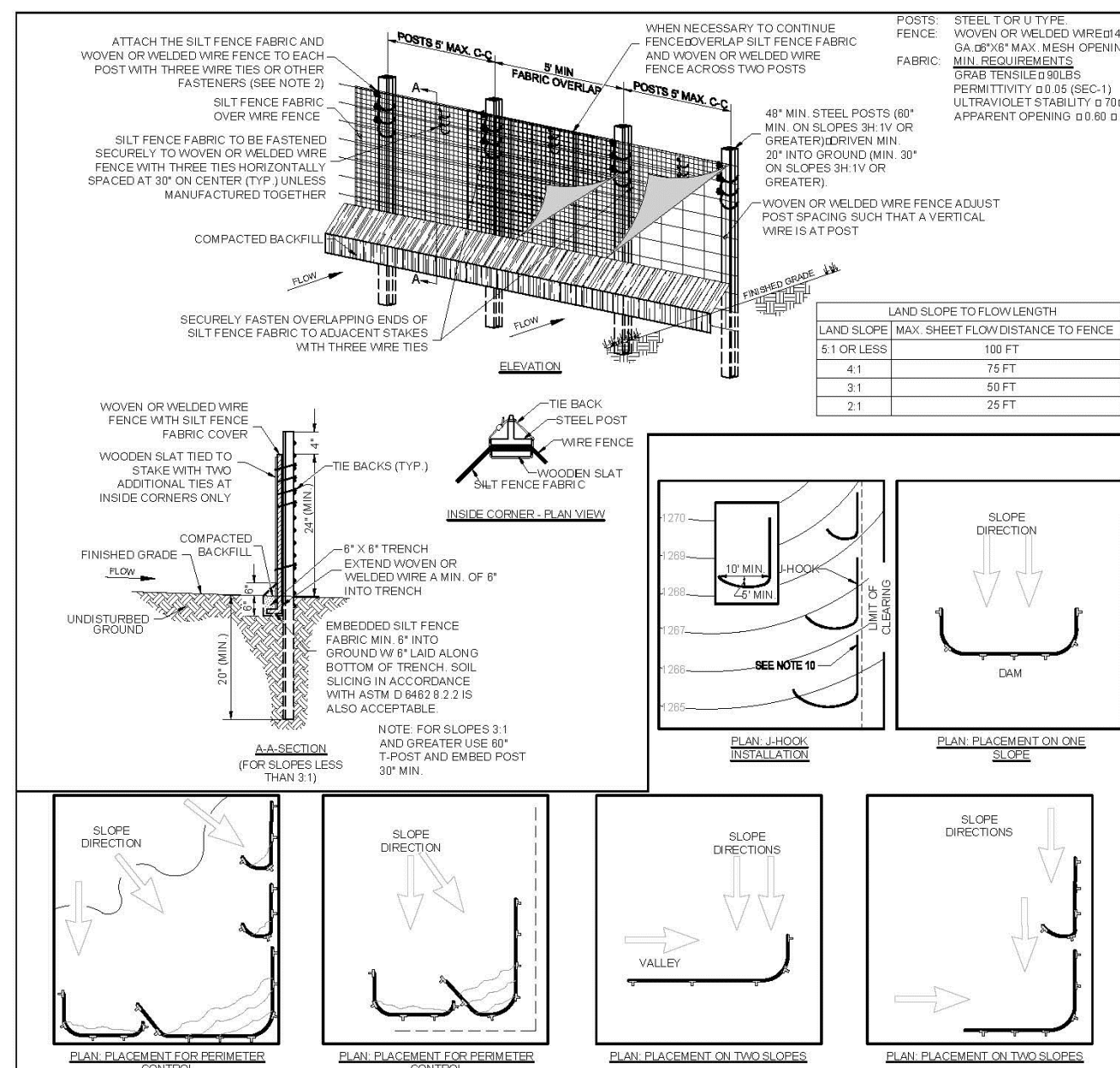
ENGINEERING DIVISION
EROSION CONTROL DETAILS
 113 W. MOUNTAIN STREET
 TEMPORARY STRAW BALE CONCRETE WASHOUT (N.T.S.)
EC2



NOTES:

1. THE SWPPP INFORMATION MAILBOX MUST BE LOCATED IN A PROMINENT, PUBLICLY ACCESSIBLE LOCATION NEAR THE MAIN ENTRANCE OF THE SITE, SUCH THAT THE DOCUMENTATION CAN BE READ WITHOUT ACCESSING THE JOBSITE, BUT NOT OBSTRUCTING VIEWS AS TO CAUSE A TRAFFIC SAFETY HAZARD.
2. ALL POSTED DOCUMENTS MUST BE MAINTAINED IN A CLEARLY READABLE CONDITION AT ALL TIMES THROUGHOUT CONSTRUCTION AND UNTIL THE NOTICE-OF-TERMINATION (NOT) IS FILED FOR THE PERMIT.
3. ALL PAGES OF NOTICES OF INTENT AND PERMIT AUTHORIZATIONS MUST BE INCLUDED. THE CONTRACTOR MAY UTILIZE ACCESSIBLE WATERPROOF FOLDERS TO STORE THESE DOCUMENTS. MAILBOX SHALL BE LOCATED OUTSIDE OF THE PUBLIC RIGHT-OF-WAY AND EASEMENTS UNLESS APPROVED BY THE GOVERNING AGENCY.
4. CONTRACTOR IS RESPONSIBLE FOR ENSURING STABILITY OF THE SWPPP INFORMATION MAILBOX.

ENGINEERING DIVISION
EROSION CONTROL DETAILS
 113 W. MOUNTAIN STREET
 SWPPP INFORMATION MAILBOX (SMALL SITE) (N.T.S.)
EC15



- INSTALLATION NOTES:**
1. MATERIALS AND INSTALLATION SHALL COMPLY WITH ASTM D 4842 LATEST EDITION.
 2. INSTALL SILT FENCE AT A FAIRLY LEVEL GRADE ALONG THE CONTOUR WITH THE ENDS CURVED UPHILL TO PROVIDE SUFFICIENT UPSTREAM STORAGE VOLUME FOR THE ANTICIPATED RUNOFF.
 3. ATTACH THE GEOTEXTILE OR FABRIC TO THE WOVEN OR WELDED WIRE FENCE WITH THREE WIRE TIES OR OTHER FASTENERS (HORIZONTALLY SPACED EVERY 30\"/>

ENGINEERING DIVISION
EROSION CONTROL DETAILS
 113 W. MOUNTAIN STREET
 SILT FENCE WITH WIRE BACKING NOTES & REQUIREMENTS
EC4

TABLE 1 Temporary Silt Fence Material Property Requirements

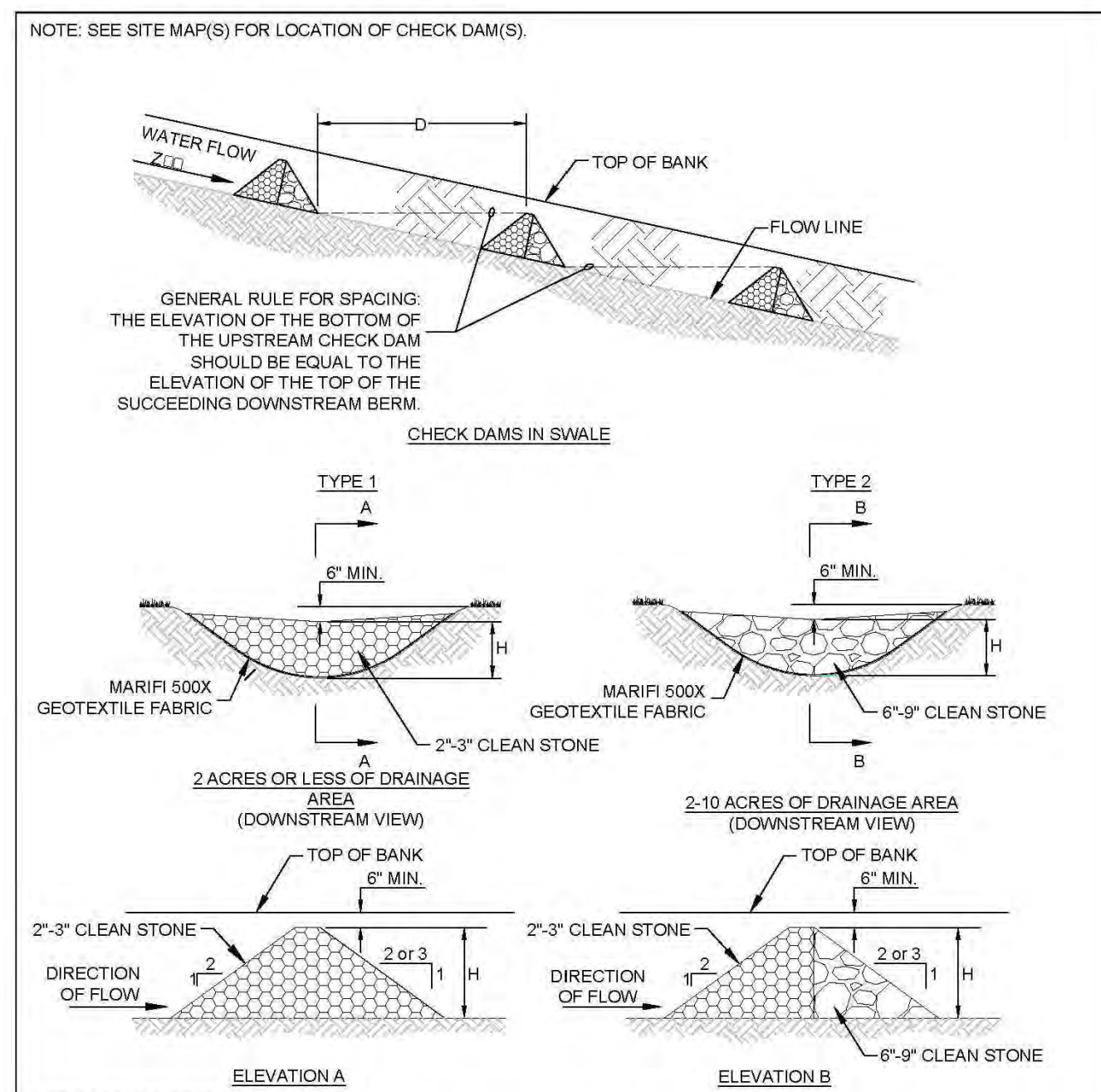
Property	Test Method	Units	Supported Silt Fence	Unsupported Silt Fence	Type of Value
Oak Strength	ASTM D 4832	N (lbs)	400 (90)	530 (90)	MARV
			400 (90)	450 (90)	MARV
Permeability ^a	ASTM D 4491	sec-1	0.05	0.05	MARV
Apparent Opening Size ^b	ASTM D 4751	mm (US Sieve #)	0.60 (30)	0.60 (30)	Max. ARV ^c
Ultraviolet Stability	ASTM D 4355	% Retained Strength	70% after 300 h of exposure	70% after 300 h of exposure	Typical

^aSilt fence support shall consist of 14 gage steel wire with a mesh spacing of 150 mm (6 in.) or prefabricated polymer mesh of equivalent strength. ^bThese default values are based on empirical evidence with a variety of sediments. For environmentally sensitive areas, a review of previous experience and/or site or regionally specific geologic tests in accordance with Test Method D 5147 should be performed by the agency to determine if a different mesh spacing is required for use in accordance with Test Method D 4832. ^cTABLE 1 TAKEN FROM ASTM D 4842-09 (2007).

- SPECIFICATIONS FOR SILT FENCE INSTALLATION**
1. MATERIALS AND INSTALLATION SHALL COMPLY WITH ASTM D 4842 LATEST EDITION.
 2. INSTALL SILT FENCE AT A FAIRLY LEVEL GRADE ALONG THE CONTOUR WITH THE ENDS CURVED UPHILL TO PROVIDE SUFFICIENT UPSTREAM STORAGE VOLUME FOR THE ANTICIPATED RUNOFF.
 3. ATTACH THE GEOTEXTILE OR FABRIC TO THE WOVEN OR WELDED WIRE FENCE WITH THREE WIRE TIES OR OTHER FASTENERS (HORIZONTALLY SPACED EVERY 30\"/>

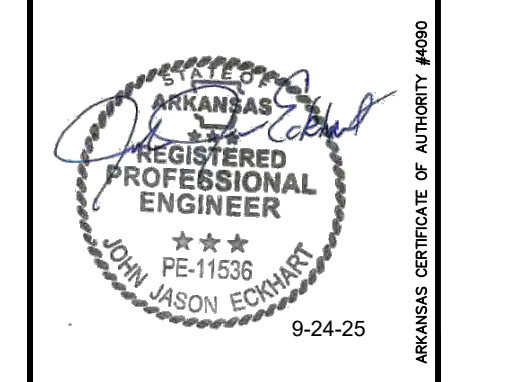
- MAINTENANCE NOTES**
1. SILT FENCES SHALL BE INSPECTED ALONG ITS ENTIRETY AND MUST BE CLEANED WHEN SEDIMENT HAS ACCUMULATED TO ONE-THIRD THE HEIGHT OF THE SILT FENCE. MAINTENANCE CLEANOUT MUST BE CONDUCTED REGULARLY TO PREVENT ACCUMULATED SEDIMENTS FROM REACHING ONE-THIRD THE HEIGHT OF THE SILT FENCE.
 2. ALL MATERIAL EXCAVATED FROM BEHIND SILT FENCE SHALL BE STOCKPILED ON AN UPLAND PORTION OF THE SITE IF SUITABLE FOR REUSE.
 3. SPECIAL ATTENTION SHOULD BE PAID TO ENSURE THAT NO UNDERMINING OF SILT FENCE HAS OCCURRED AND THAT NO BYPASS IS OCCURRING AT JOINING SECTIONS.
 4. IF EXCESS SEDIMENT IS ACCUMULATING IN ANY SECTION OF SILT FENCE, THE CONTRACTOR SHOULD IMPLEMENT ADDITIONAL UPSTREAM STABILIZATION MEASURES OR ADDITIONAL BMPs (PENDING CEC APPROVAL) TO PREVENT EXCESSIVE BUILDUP ON SILT FENCE.
 5. SILT FENCES SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED.

ENGINEERING DIVISION
EROSION CONTROL DETAILS
 113 W. MOUNTAIN STREET
 SILT FENCE WITH WIRE BACKING NOTES & REQUIREMENTS
EC4B



- INSTALLATION NOTES:**
1. STONES SHOULD BE PLACED UP THE CHANNEL BANKS TO PREVENT WATER FROM CUTTING AROUND THE DITCH CHECK.
 2. INSTALLATION SHALL BE PLACED EITHER BY HAND OR MECHANICALLY (NOT JUST DUMPED) TO ACHIEVE COMPLETE COVERAGE OF THE DITCH AND ENSURE THE CENTER OF THE DAM IS LOWER THAN THE EDGES.
 3. MAXIMUM SPACING BETWEEN MULTIPLE DAMS SHOULD BE SUCH THAT THE TOE OF THE UPSTREAM CHECK IS THE SAME AS THE TOP OF THE DOWNSTREAM CHECK.
- INSPECTION NOTES:**
4. SEDIMENT SHOULD BE REMOVED WHEN IT REACHES 1/2 THE ORIGINAL CHECK HEIGHT.
 5. IN THE CASE OF GRASS-LINED DITCHES OR SWALES, ROCK DITCH CHECKS SHOULD BE REMOVED WHEN THE GRASS HAS MATURED SUFFICIENTLY TO PROTECT THE DITCH OR SWALE IF THE SLOPE IS 4% OR LESS.
 6. THE AREA BENEATH THE ROCK DITCH CHECKS SHOULD BE SEEDED AND MULCHED IMMEDIATELY AFTER THE CHECK DAM REMOVAL.

ENGINEERING DIVISION
EROSION CONTROL DETAILS
 113 W. MOUNTAIN STREET
 ROCK CHECK DAM (N.T.S.)
EC17



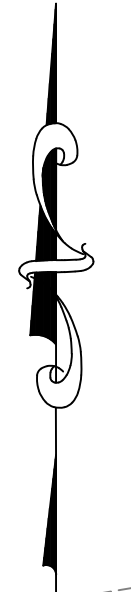
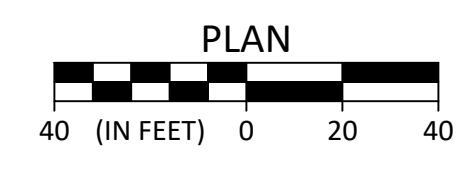
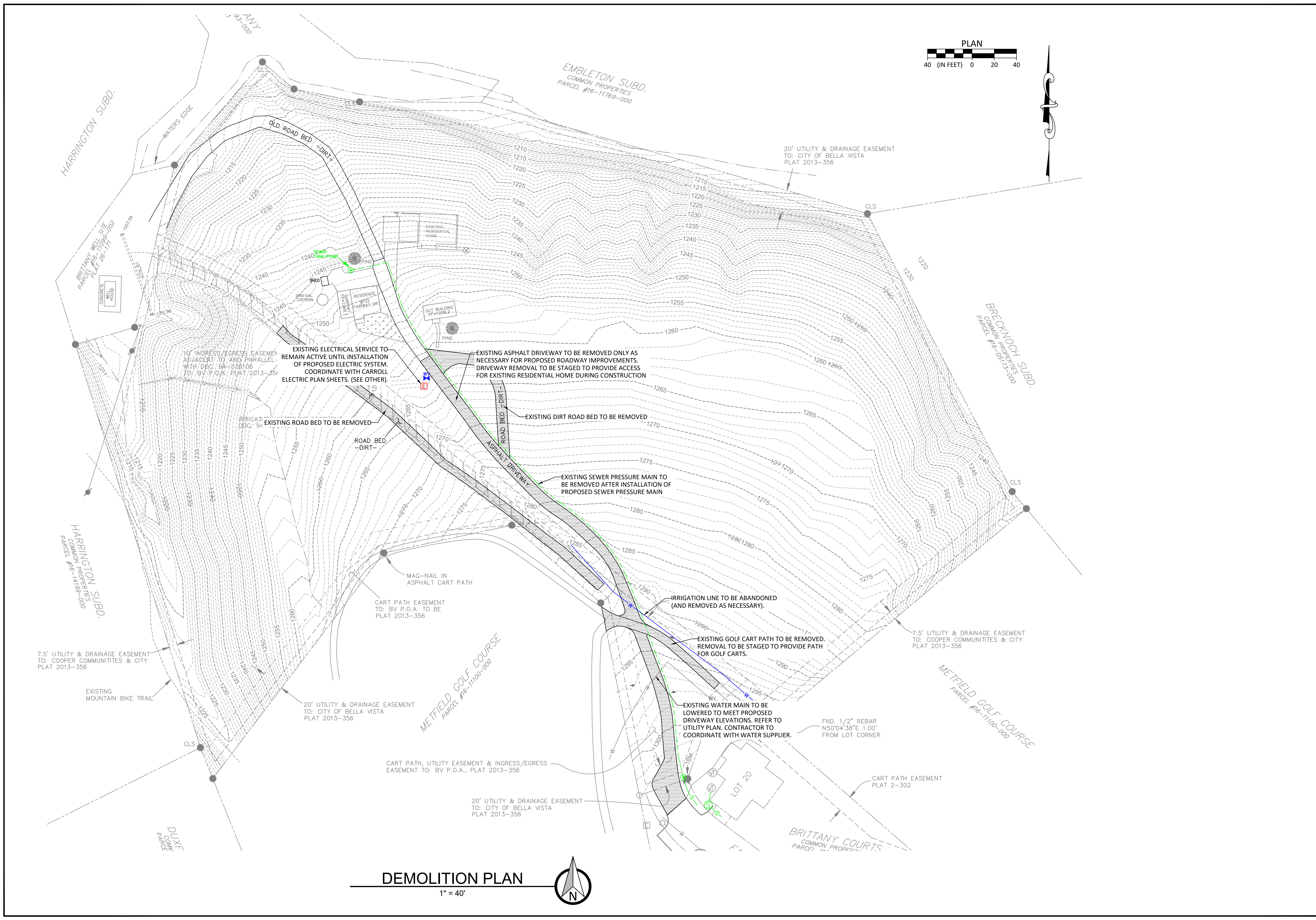
REVISIONS

NO.	DESCRIPTION	DATE	BY

ENGINEERING DIVISION
EROSION CONTROL
 113 W. MOUNTAIN STREET
EC1

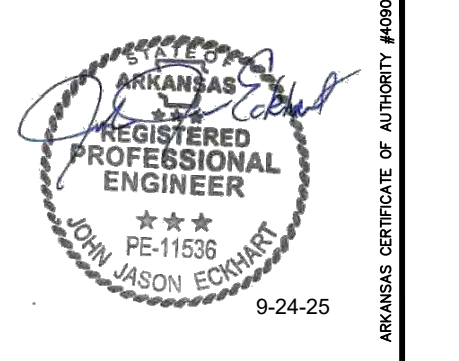
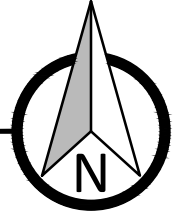
ENGINEERING DIVISION
EROSION CONTROL DETAILS
 113 W. MOUNTAIN STREET
 SILT FENCE WITH WIRE BACKING NOTES & REQUIREMENTS
EC4

ENGINEERING DIVISION
EROSION CONTROL
 113 W. MOUNTAIN STREET
 ROCK CHECK DAM (N.T.S.)
EC17



DEMOLITION PLAN

1" = 40'



NO.	REVISIONS	DESCRIPTION	BY	DATE

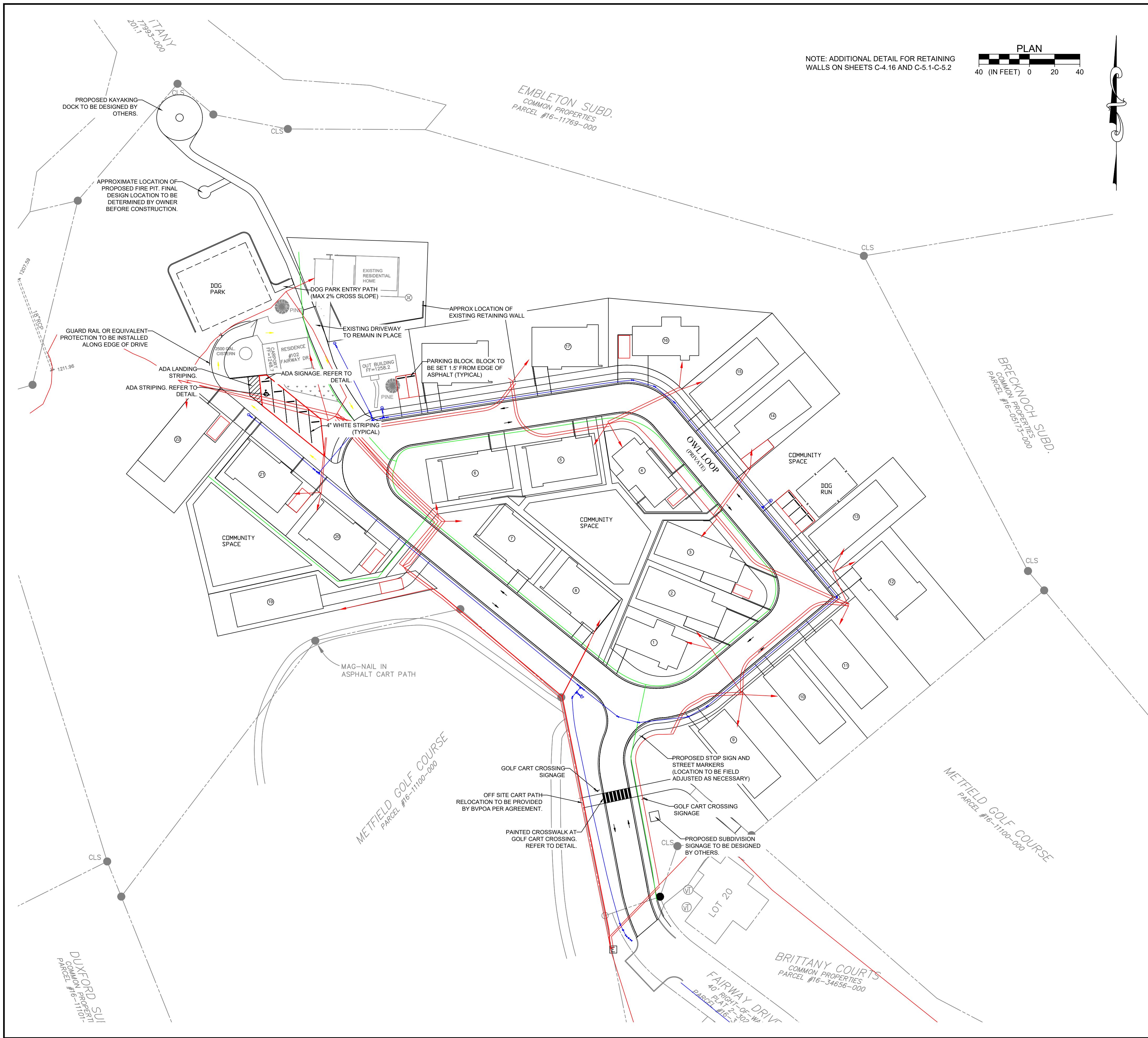
OWL COMMONS

**102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714**

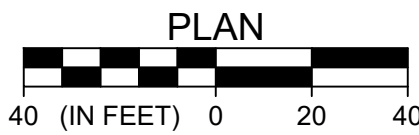
**C-3.2
DEMOLITION PLAN**

PROJECT NUMBER:
230082
DATE:
08/28/2025





NOTE: ADDITIONAL DETAIL FOR RETAINING WALLS ON SHEETS C-4.16 AND C-5.1-C-5.2



- LEGEND**
- PARKING ZONES
 - TRAFFIC CIRCULATION PATTERN NOTATION
 - - - PEDESTRIAN CIRCULATION PATTERN NOTATION
 - W - WATER MAIN
 - SS - 3"Ø SEWER PRESSURE MAIN
 - PROPOSED UTILITY / ACCESS EASEMENTS
 - PROPOSED ACCESS EASEMENT
 - PROPOSED DRAINAGE EASEMENTS
 - EXISTING EASEMENTS
 - PROPOSED PROPERTY LINES
 - EXISTING PROPERTY LINES
 - X - PROPOSED FENCING

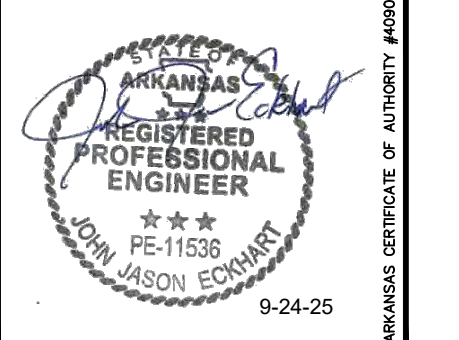
OPEN SPACE REQUIREMENTS:
 TOTAL ACREAGE: 7.69 AC
 REQUIRED ACREAGE FOR OPEN SPACE: 1.54 AC
 PROVIDED OPEN SPACE: 4.06 AC

COMMON OPEN SPACE SHALL BE LAND DEVOTED TO CONSERVATION OR RECREATIONAL PURPOSES AND SHALL BE DESIGNATED TO REMAIN UNDEVELOPED, EXCEPT WITHIN THE PROVISIONS PROVIDED BELOW. COMMON OPEN SPACE SHALL BE OPEN TO OWNERS, TENANTS, AND CUSTOMERS WITHIN THE PLANNED ZONING DISTRICT, AND SHALL ADHERE TO THE FOLLOWING REQUIREMENTS:

1. MINIMUM AREA. A MINIMUM OF 20 PERCENT OF THE TOTAL PROJECT AREA, EXCLUSIVE OF RIGHTS-OF-WAY, SHALL BE DEVOTED TO COMMON OPEN SPACE.
2. OPEN SPACE TO BE DISTRIBUTED THROUGHOUT THE PLANNED ZONING DISTRICT IN RELATIONSHIP TO THE DWELLING UNITS OR OTHER USE AREAS THAT ARE INTENDED TO BE SERVED BY THE COMMON OPEN SPACE.
3. OPEN SPACE CALCULATIONS MAY INCLUDE (IF PRESENT): WETLANDS AND WATER BODIES; INCLUDING THE NORMAL POOL ELEVATION SURFACE AREA OF RETENTION PONDS; ACTIVE DETENTION PONDS; IDENTIFIED FLOODPLAINS AND FLOODWAYS IN THE CURRENTLY EFFECTIVE FEMA FLOOD MAPS ADOPTED BY THE CITY; VEGETATED AND LANDSCAPED AREAS, EXCLUDING REQUIRED PARKING LOT LANDSCAPING REQUIREMENTS; NATURAL BUFFER AREAS BETWEEN THE PLANNED ZONING DISTRICT AND ADJACENT PROPERTIES; PUBLIC PLAZAS AND HARD SURFACE RECREATION AREAS; AND PEDESTRIAN PATHS, TRAILS, AND SIDEWALKS, EXCLUSIVE OF THOSE REQUIRED BY THE SUBDIVISION CODE.
4. RETAINING WALLS CONNECTED TO THE STRUCTURE ARE TO BE PERMITTED SEPARATELY AFTER SELECTION OF HOUSES BY DEVELOPER AND ARE SHOWN FOR REFERENCE ONLY.

STRUCTURE DATA				
NO.	HOUSE TYPE* (AS SHOWN)	HEIGHT (AS SHOWN)	MAXIMUM HEIGHT	BEDROOMS*
1	TYPE B1	12'-2"	16'-3 1/8"	1 BED
2	TYPE A1	16'-3 1/8"	16'-3 1/8"	1 BED
3	TYPE A1	16'-3 1/8"	16'-3 1/8"	1 BED
4	TYPE B1	12'-2"	16'-3 1/8"	1 BED
5	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
6	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
7	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
8	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
9	TYPE A2	14'-10.5"	16'-3 1/8"	2 BED
10	TYPE A2	14'-10.5"	16'-3 1/8"	2 BED
11	TYPE A2	14'-10.5"	16'-3 1/8"	2 BED
12	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
13	TYPE A2	14'-10.5"	16'-3 1/8"	2 BED
14	TYPE A1	16'-3 1/8"	16'-3 1/8"	1 BED
15	TYPE A2	14'-10.5"	16'-3 1/8"	2 BED
16	TYPE B1	12'-2"	16'-3 1/8"	1 BED
17	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
18	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
19	TYPE A2	14'-10.5"	16'-3 1/8"	2 BED
20	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
21	TYPE B2	13'-4.25"	16'-3 1/8"	2 BED
22	TYPE A2	14'-10.5"	16'-3 1/8"	2 BED

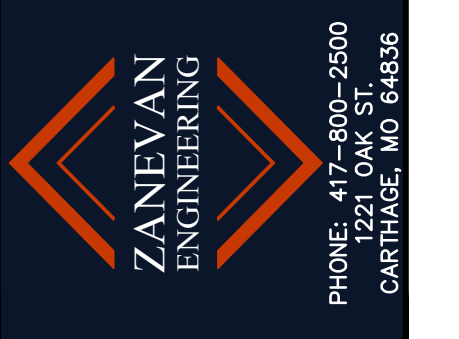
HOUSE TYPES LISTED ABOVE ARE AS SHOWN ON THE PLANS AS EXAMPLES ONLY. IT IS THE INTENT OF THE DEVELOPMENT TO ALLOW FOR A SELECTION BETWEEN THE 6 HOUSE TYPES (A1, A2, A3, B1, B2, B3) FOR EACH LOT, AS AVAILABLE ACCORDING TO SETBACK, EASEMENT, AND UTILITY REQUIREMENTS. MAXIMUM HOUSE HEIGHT OF 16'-3 1/8". (1 BED: A1, A3, B1, B3; 2 BED: A2, B2)

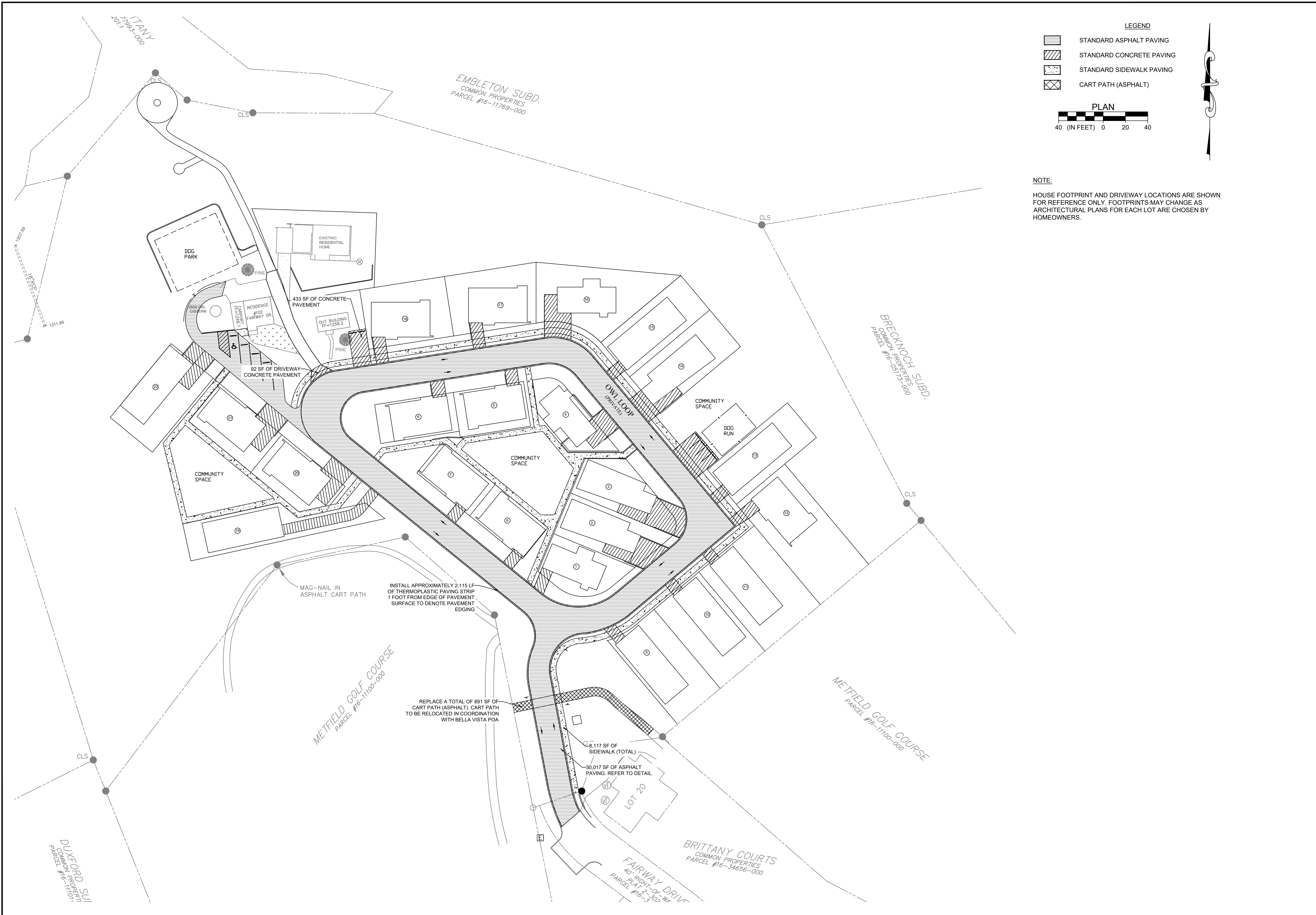


NO.	DESCRIPTION	DATE	BY

OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.0
 MASTER PLAN
 OVERVIEW





- LEGEND**
- STANDARD ASPHALT PAVING
 - STANDARD CONCRETE PAVING
 - STANDARD SIDEWALK PAVING
 - CART PATH (ASPHALT)



NOTE:
HOUSE FOOTPRINT AND DRIVEWAY LOCATIONS ARE SHOWN FOR REFERENCE ONLY. FOOTPRINTS MAY CHANGE AS ARCHITECTURAL PLANS FOR EACH LOT ARE CHOSEN BY HOMEOWNERS.



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

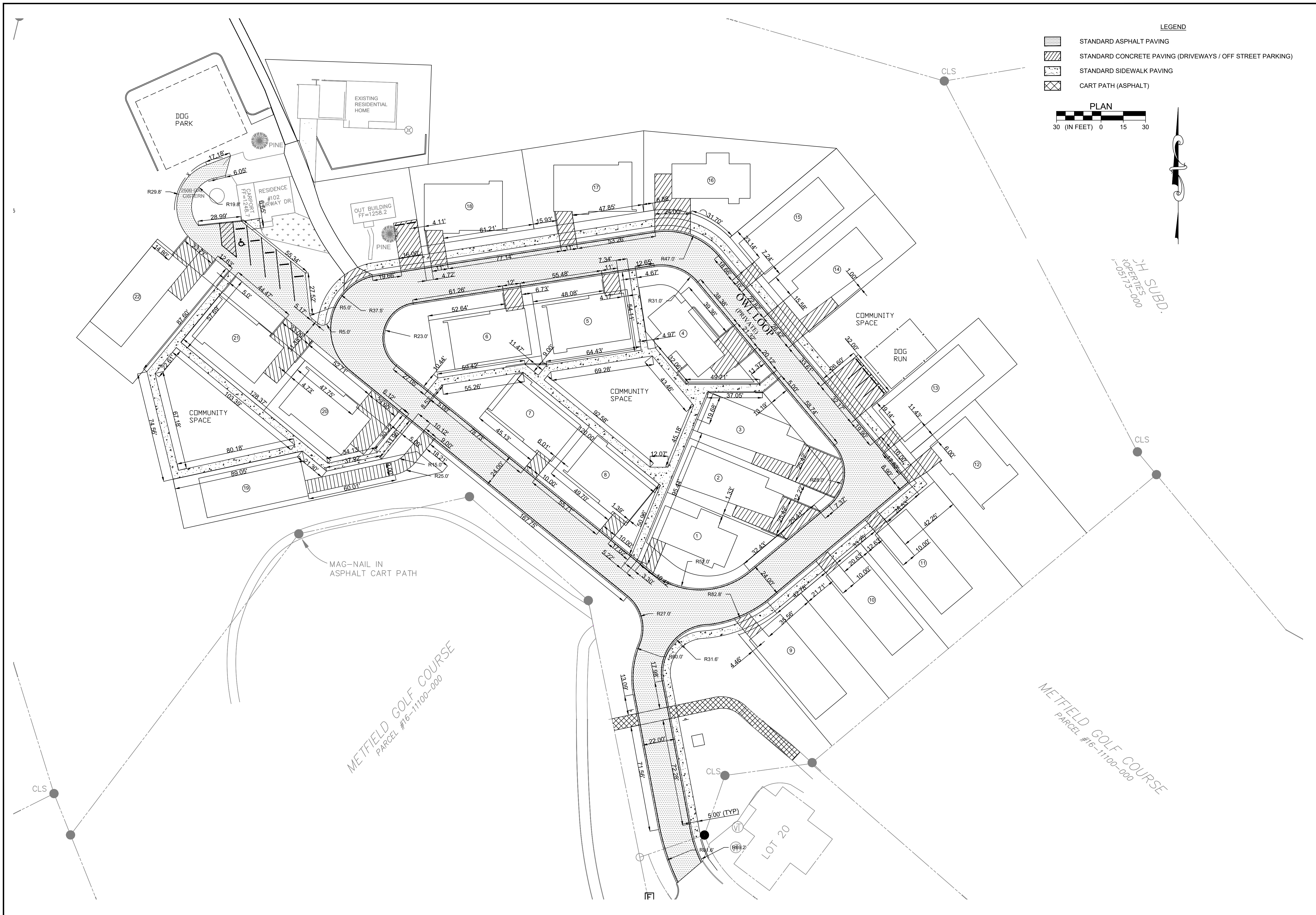
102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

**C-4.1
PAVING PLAN**

PROJECT NUMBER:
230082
DATE:
08/28/2025



PHONE: 417-600-2500
1221 OAK ST.
CAPRICE, MO 64636

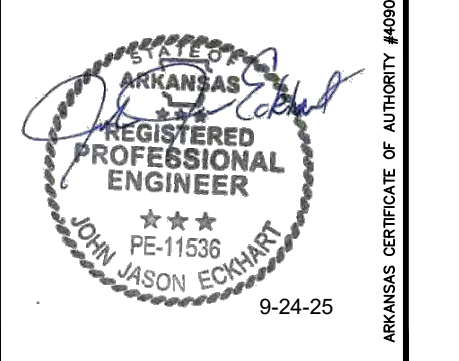


LEGEND

- STANDARD ASPHALT PAVING
- STANDARD CONCRETE PAVING (DRIVEWAYS / OFF STREET PARKING)
- STANDARD SIDEWALK PAVING
- CART PATH (ASPHALT)

PLAN

30 (IN FEET) 0 15 30



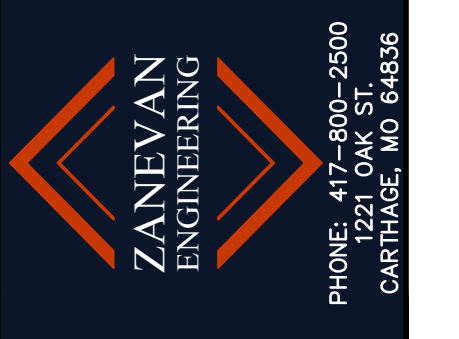
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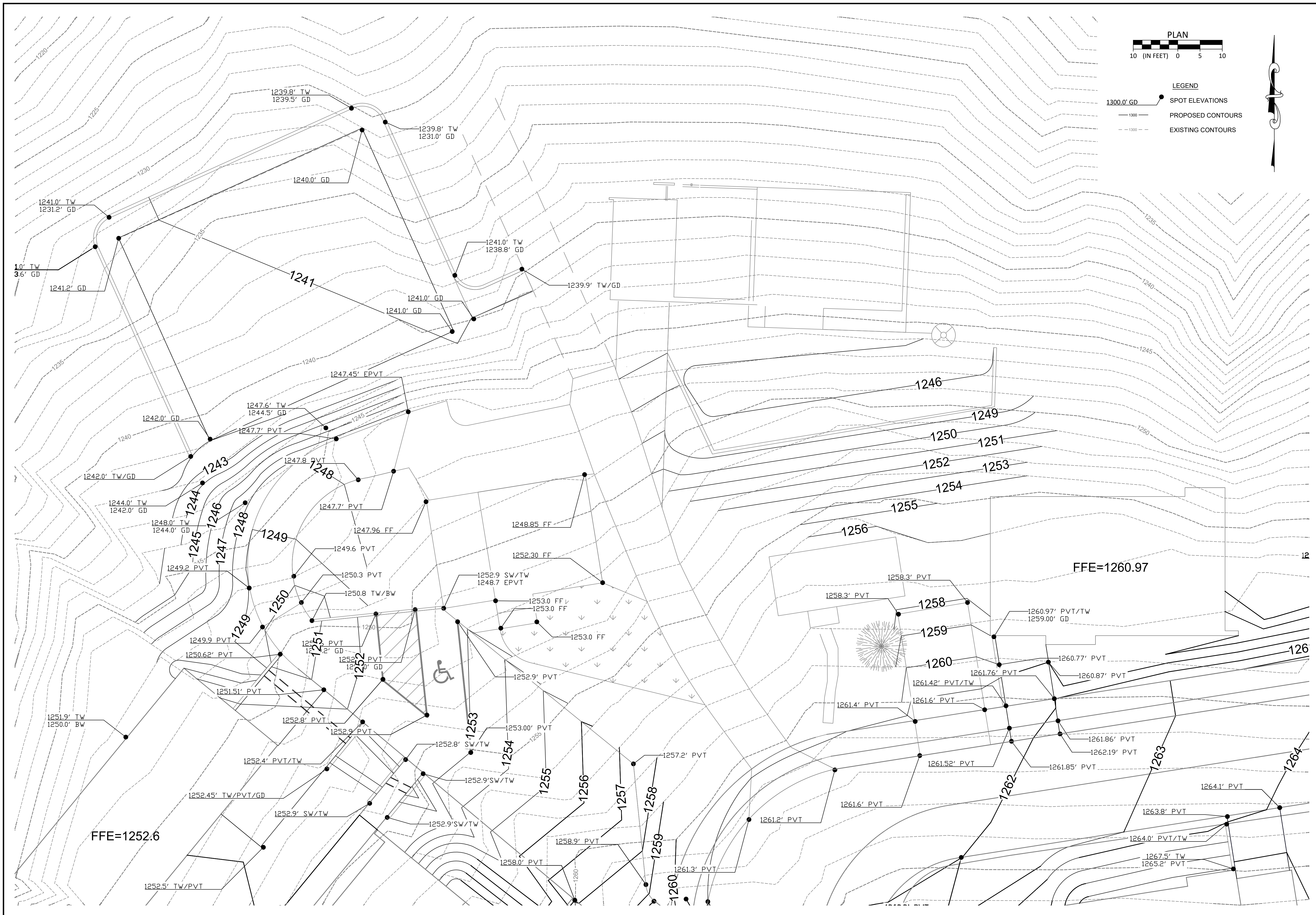
OWL COMMONS

102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.2
 DIMENSIONAL PLAN

PROJECT NUMBER:
 230082
 DATE:
 08/28/2025





LEGEND

- 1300.0' GD SPOT ELEVATIONS
- PROPOSED CONTOURS
- - - EXISTING CONTOURS



NO.	REVISIONS	DESCRIPTION	BY	DATE

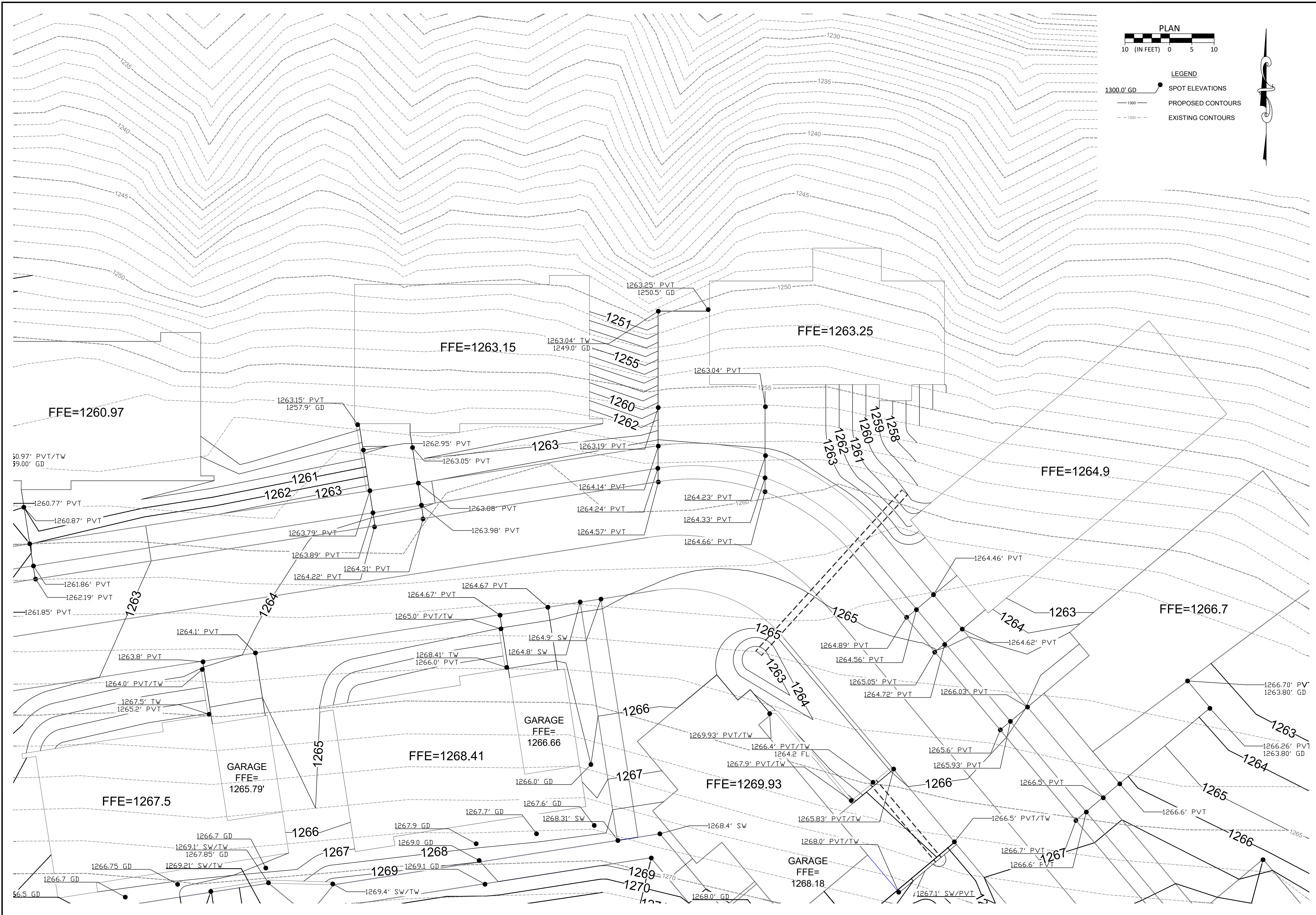
OWL COMMONS

C-4.4
GRADING PLAN
NORTHWEST

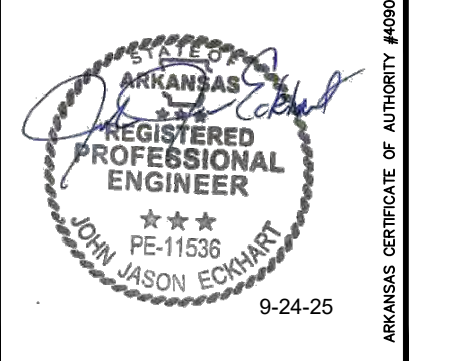
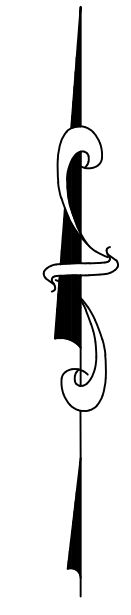
ZANEVAN ENGINEERING
 PHONE: 417-600-2500
 1221 OAK ST.
 CARTRAGE, MO 64636

PROJECT NUMBER:
 230082
 DATE:
 08/28/2025

102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714



LEGEND
 1300.0' GD SPOT ELEVATIONS
 — 1300 — PROPOSED CONTOURS
 - - 1300 - - EXISTING CONTOURS



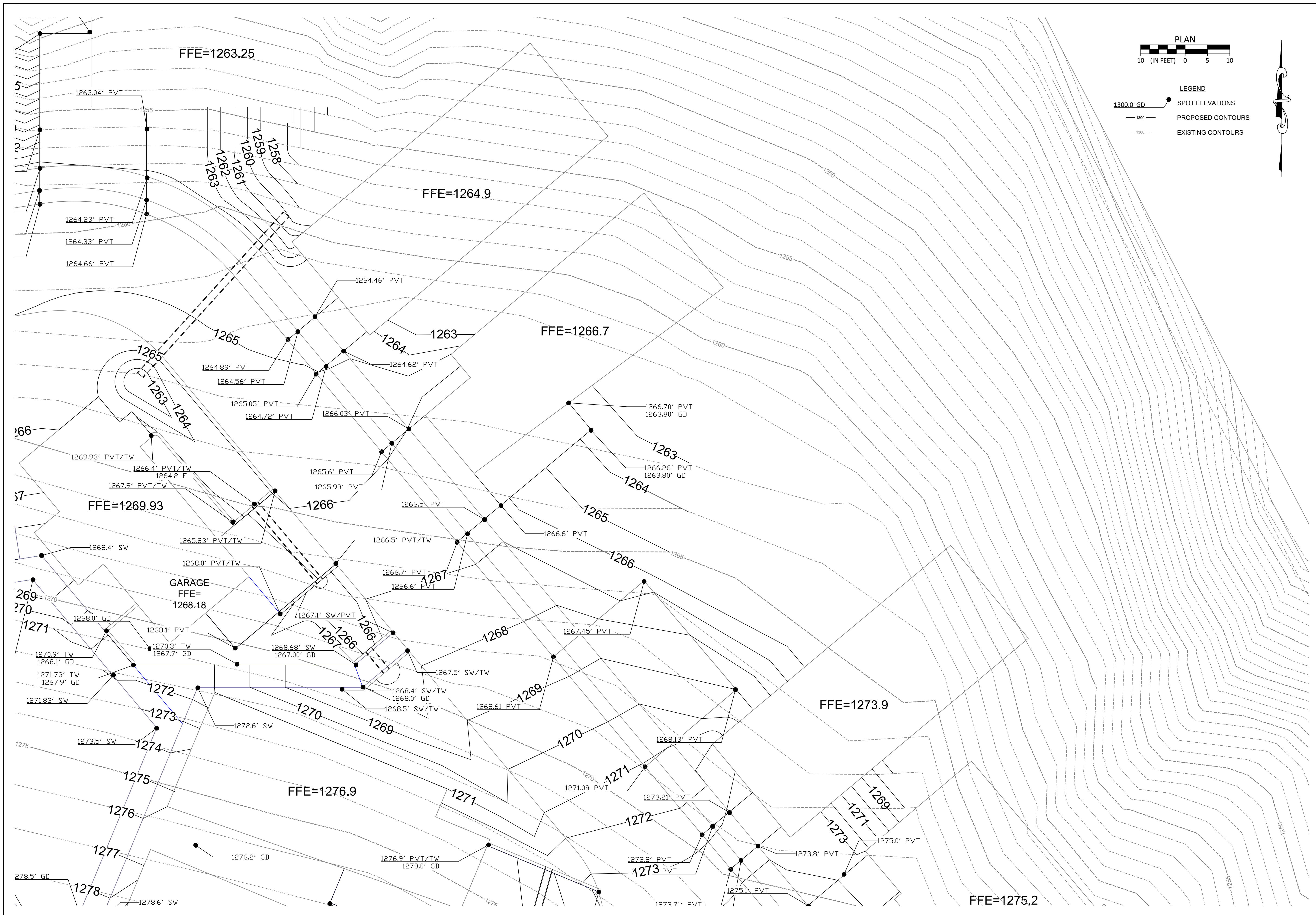
NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.5
 GRADING PLAN
 NORTH
 PROJECT NUMBER: 230082
 DATE: 08/28/2025



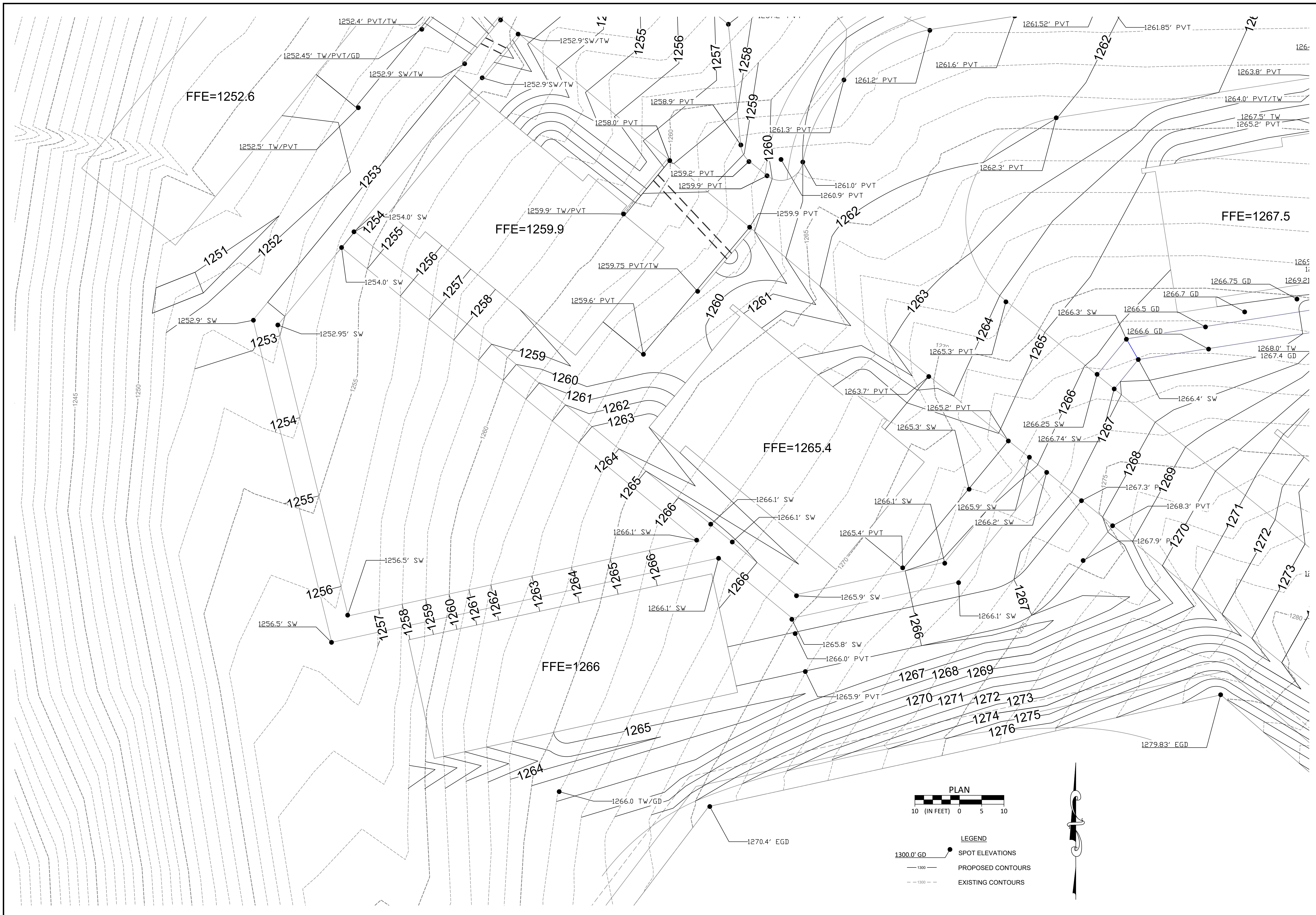


NO.	REVISIONS	BY	DATE

OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.6
 GRADING PLAN
 NORTHEAST
 PROJECT NUMBER: 230082
 DATE: 08/28/2025

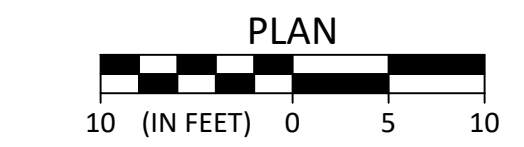




NO.	DESCRIPTION	BY	DATE

OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

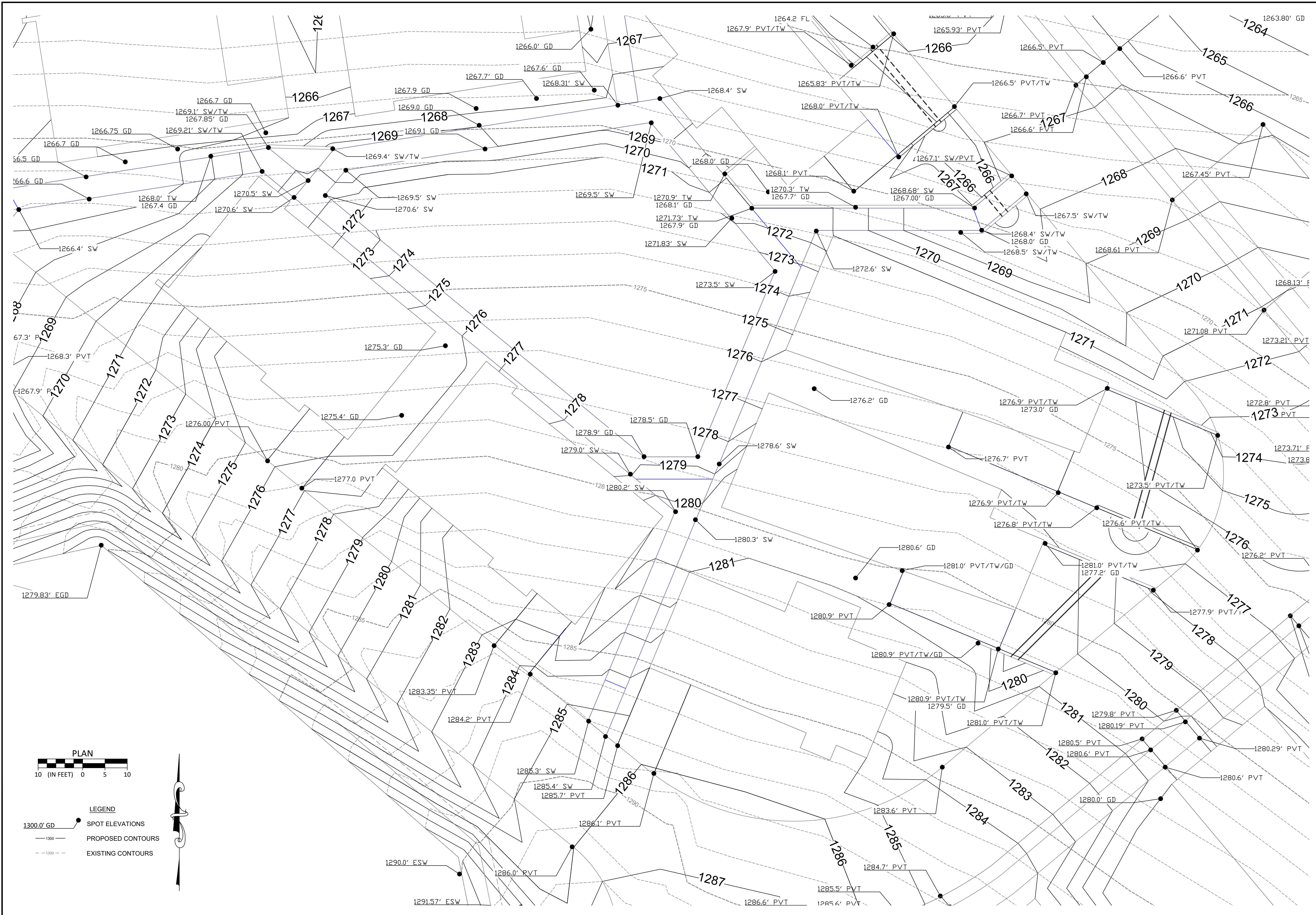
C-4.7
 GRADING PLAN WEST



LEGEND

- 1300.0' GD (Symbol: Spot Elevation)
- 1300 — (Symbol: Proposed Contour)
- - 1300 - - (Symbol: Existing Contour)
- SPOT ELEVATIONS
- PROPOSED CONTOURS
- - EXISTING CONTOURS



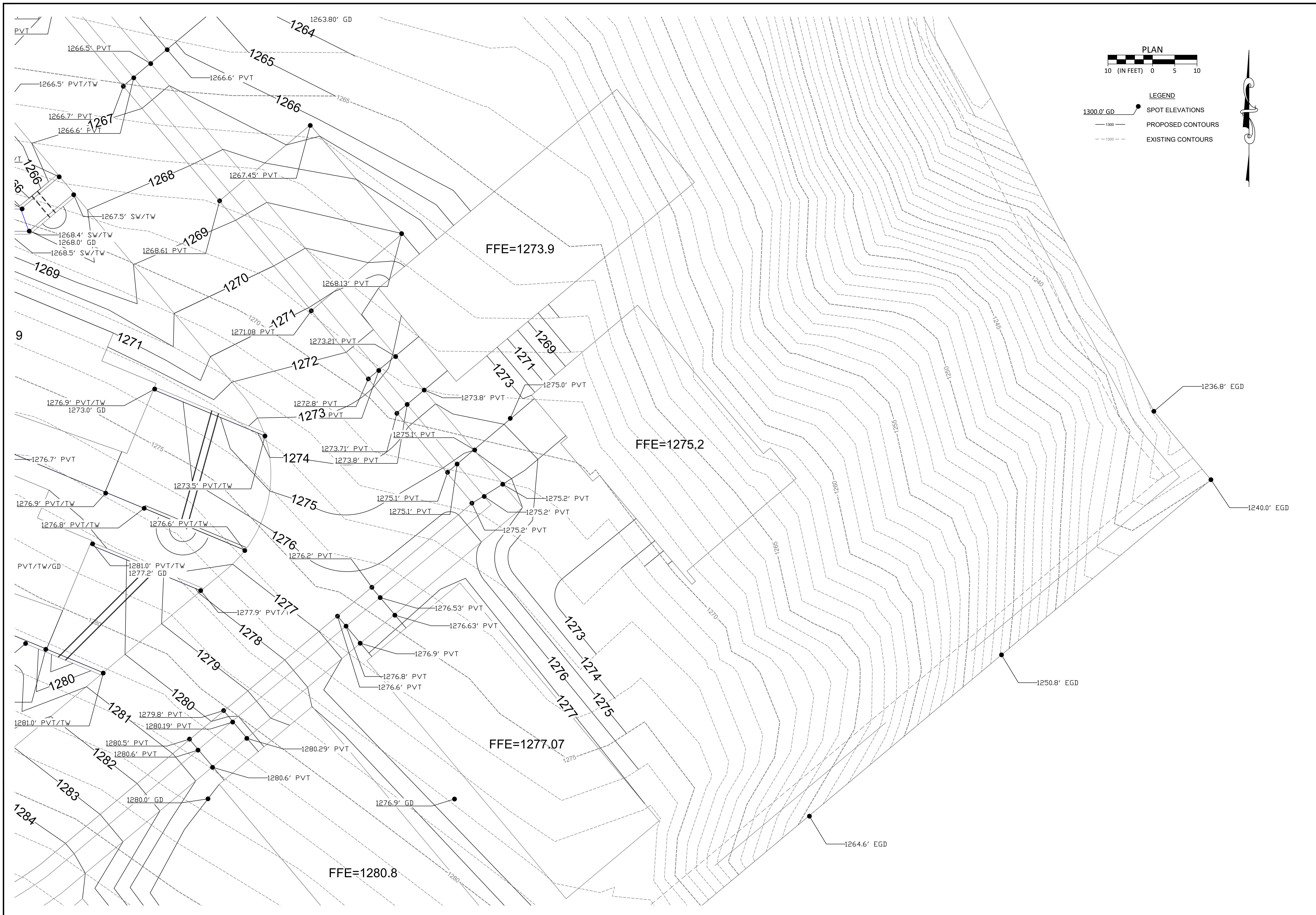


NO.	DESCRIPTION	DATE	BY

OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.8
 GRADING PLAN
 CENTRAL
 PROJECT NUMBER: 230082
 DATE: 08/28/2025



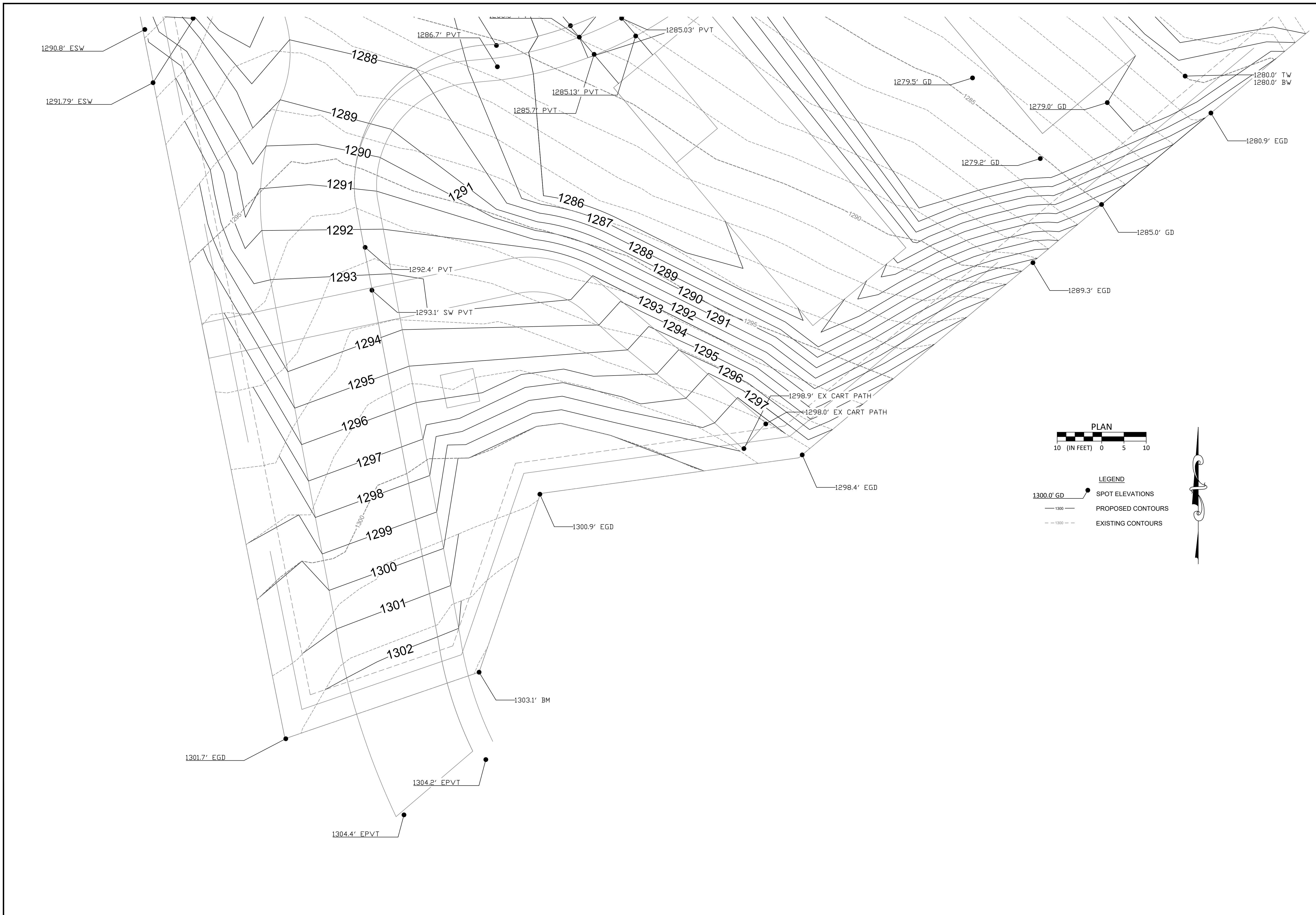


NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.9
 GRADING PLAN EAST
 PROJECT NUMBER: 230082
 DATE: 08/28/2025





NO.	REVISIONS	DESCRIPTION	BY	DATE

PLAN

10 (IN FEET) 0 5 10

LEGEND

- 1300.0' GD SPOT ELEVATIONS
- 1300 — PROPOSED CONTOURS
- - 1300 - - EXISTING CONTOURS

OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.10
 GRADING PLAN SOUTH

PROJECT NUMBER:
 230082
 DATE:
 08/28/2025



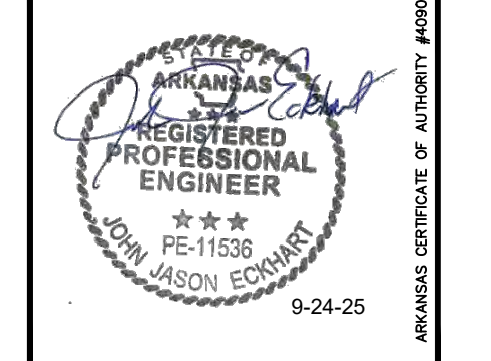


SOIL TYPES

- CVF, CLARKSVILLE EXTREMELY GRAVELLY SILT LOAM, 12 TO 50 PERCENT SLOPES, RATING = SC
- NFD, NIXA VERY GRAVELLY SILT LOAM, 8 TO 12 PERCENT SLOPES, RATING = PT

PLAN

40 (IN FEET) 0 20 40



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

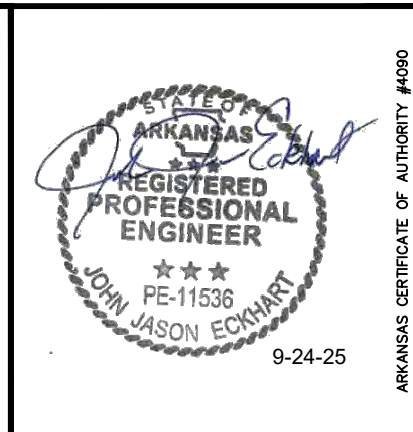
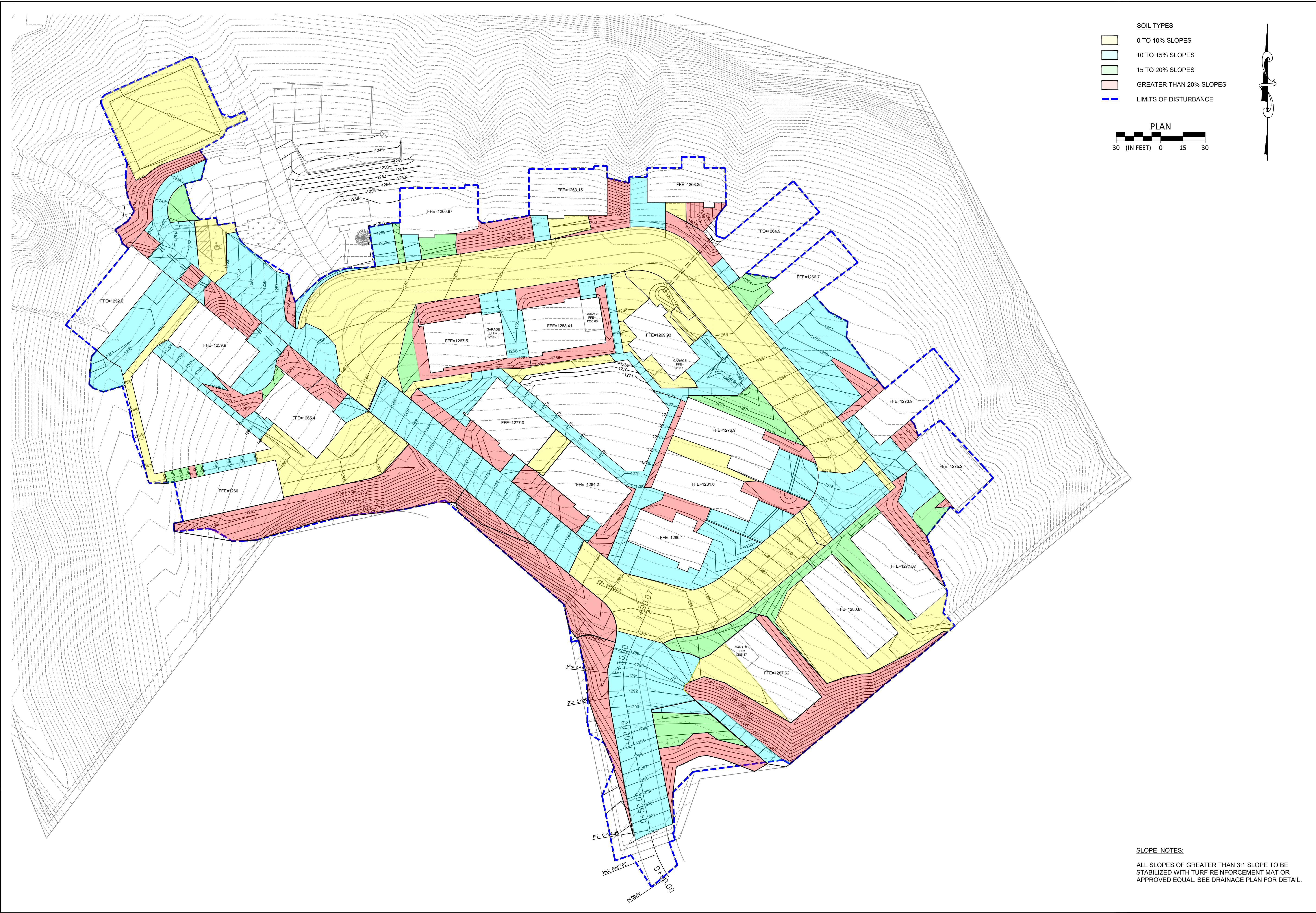
102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.12
 UNIFIED SOILS TYPES

PROJECT NUMBER: 230082
 DATE: 08/28/2025

ZANEVAN ENGINEERING

PHONE: 417-800-2500
 1221 OAK ST.
 CARPAGE, MO 64636



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

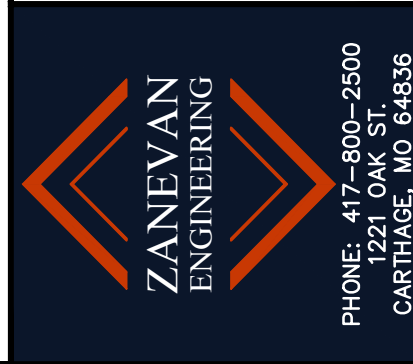
102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.13

GRADING SLOPES

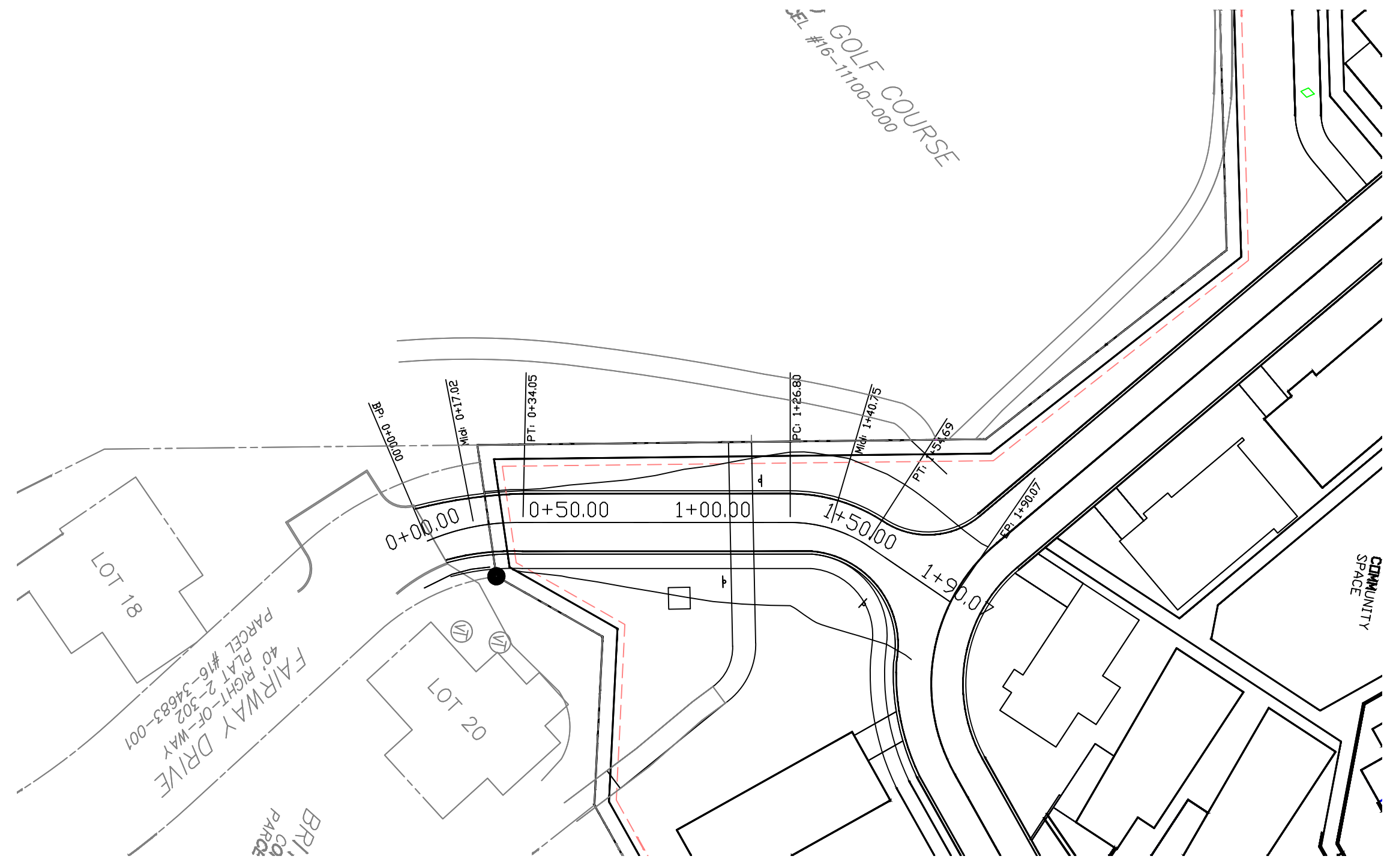
PROJECT NUMBER:
 230082

DATE:
 08/28/2025



SLOPE NOTES:

ALL SLOPES OF GREATER THAN 3:1 SLOPE TO BE STABILIZED WITH TURF REINFORCEMENT MAT OR APPROVED EQUAL. SEE DRAINAGE PLAN FOR DETAIL.

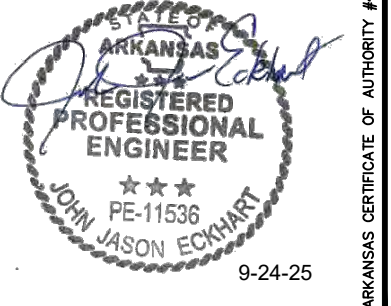
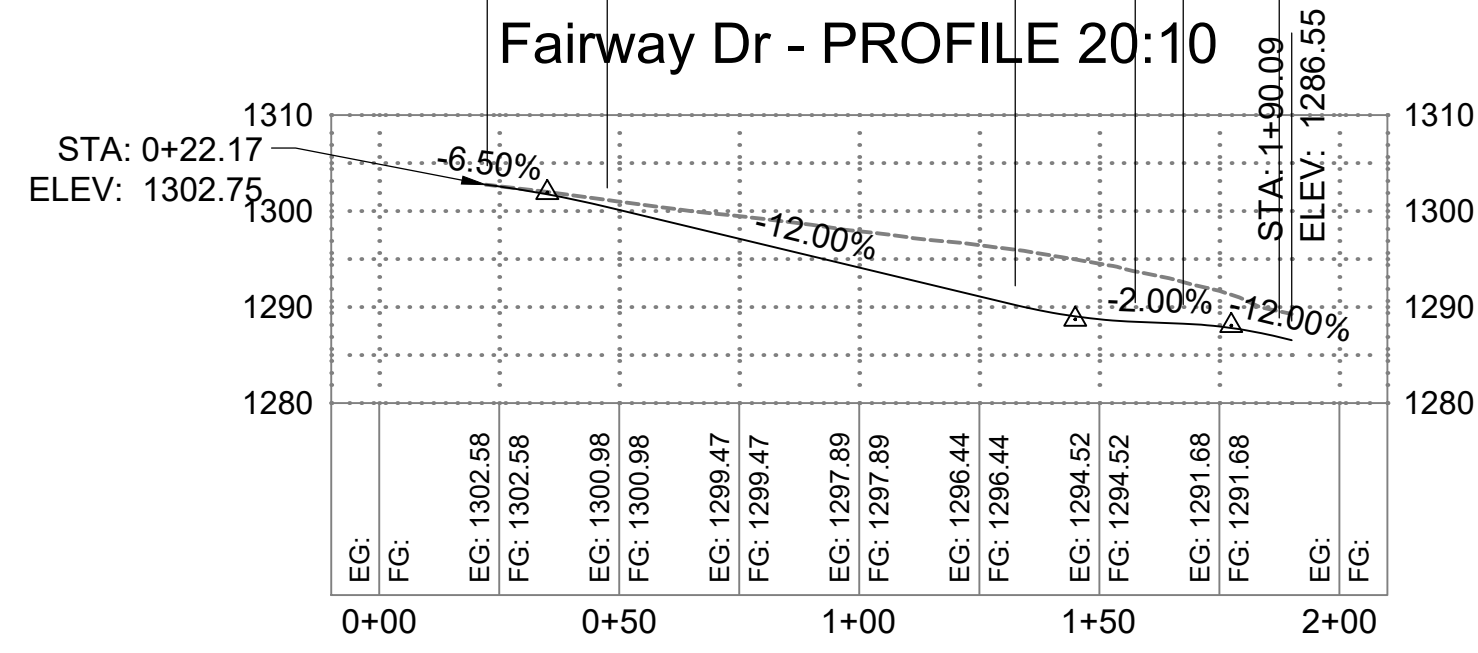


PVI STA: 1+44.98
 PVI ELEV: 1288.72
 LOW PT STA: 1+57.48
 LOW PT ELEV: 1288.47
 K: 2.50
 LVC: 25.00

BVCS: 1+32.48
 BVCE: 1290.22
 EVCS: 1+57.48
 EVCE: 1288.47

PVI STA: 0+35.00
 PVI ELEV: 1301.91
 HIGH PT STA: 0+22.50
 HIGH PT ELEV: 1302.73
 K: 4.55
 LVC: 25.00
 BVCS: 0+22.50
 BVCE: 1302.73
 EVCS: 0+47.50
 EVCE: 1300.41

PVI STA: 1+77.48
 PVI ELEV: 1288.07
 HIGH PT STA: 1+67.48
 HIGH PT ELEV: 1288.27
 K: 2.00
 LVC: 20.00
 BVCS: 1+67.48
 BVCE: 1288.27
 EVCS: 1+87.48
 EVCE: 1286.87



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

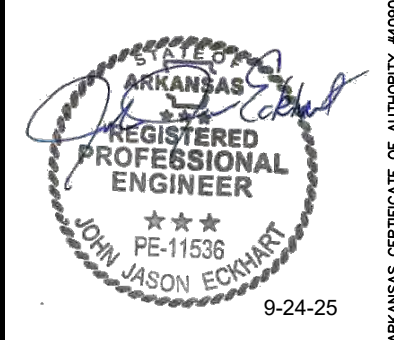
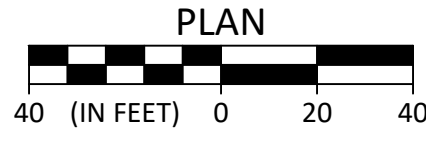
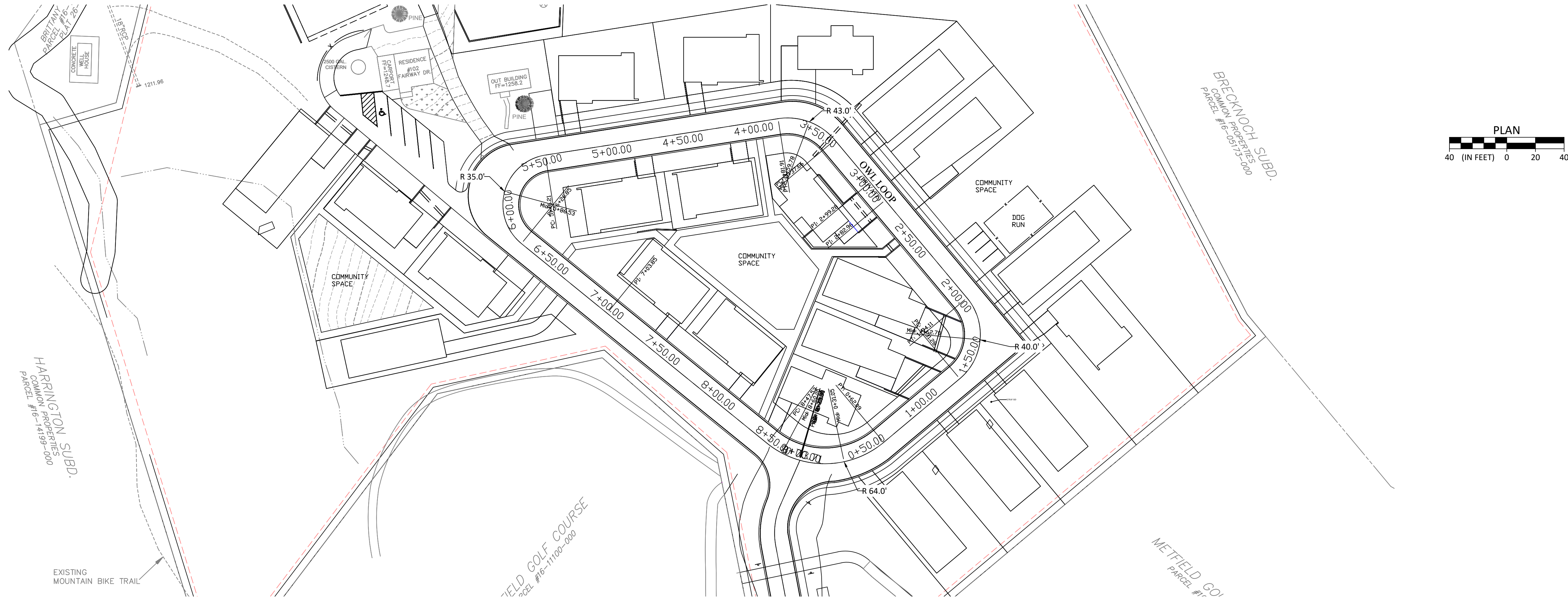
102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-4.14
 FAIRWAY DR PROFILE

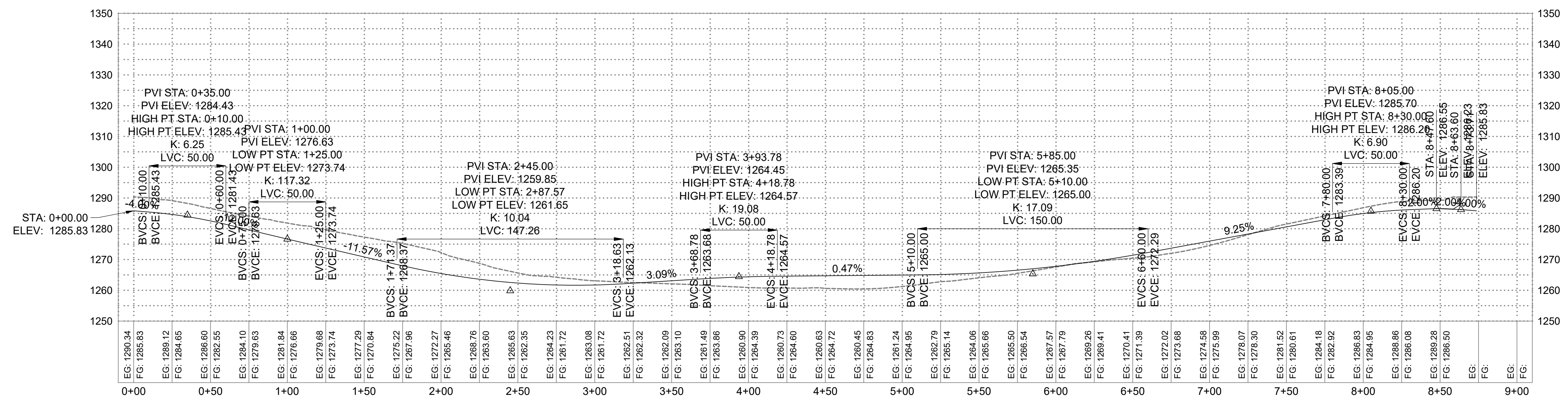
PROJECT NUMBER:
 230082
 DATE:
 08/28/2025



PHONE: 417-600-2500
 1221 OAK ST.
 CAPRICE, MO 64636



NO.	REVISIONS	DESCRIPTION	BY	DATE

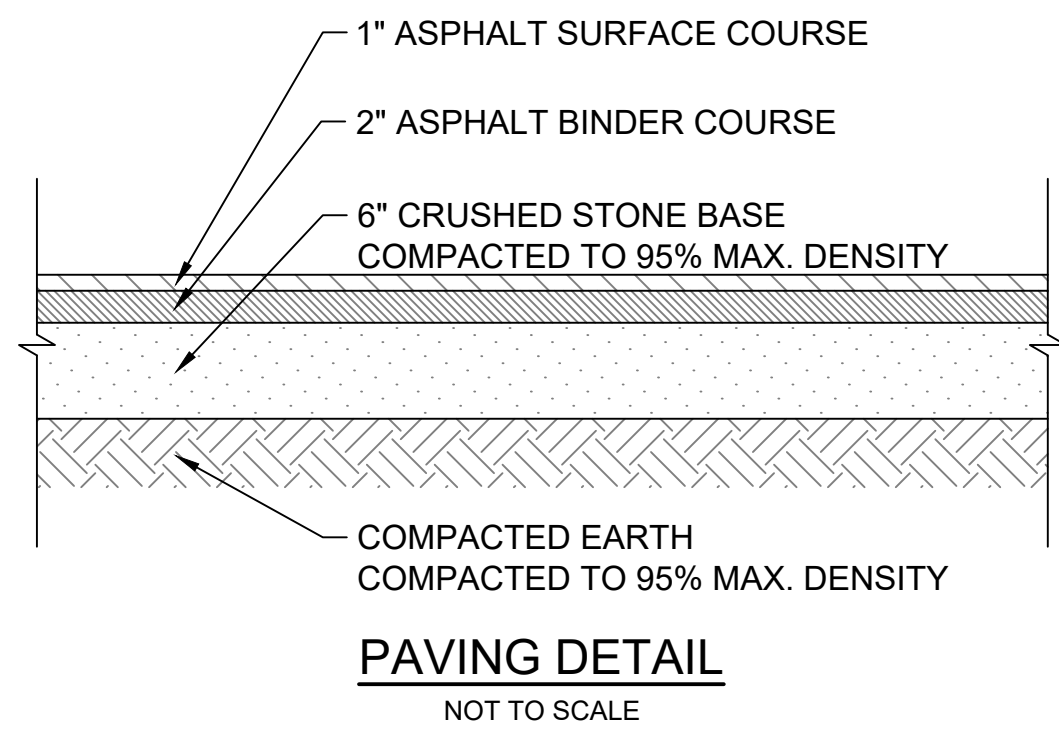


OWL COMMONS
 102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

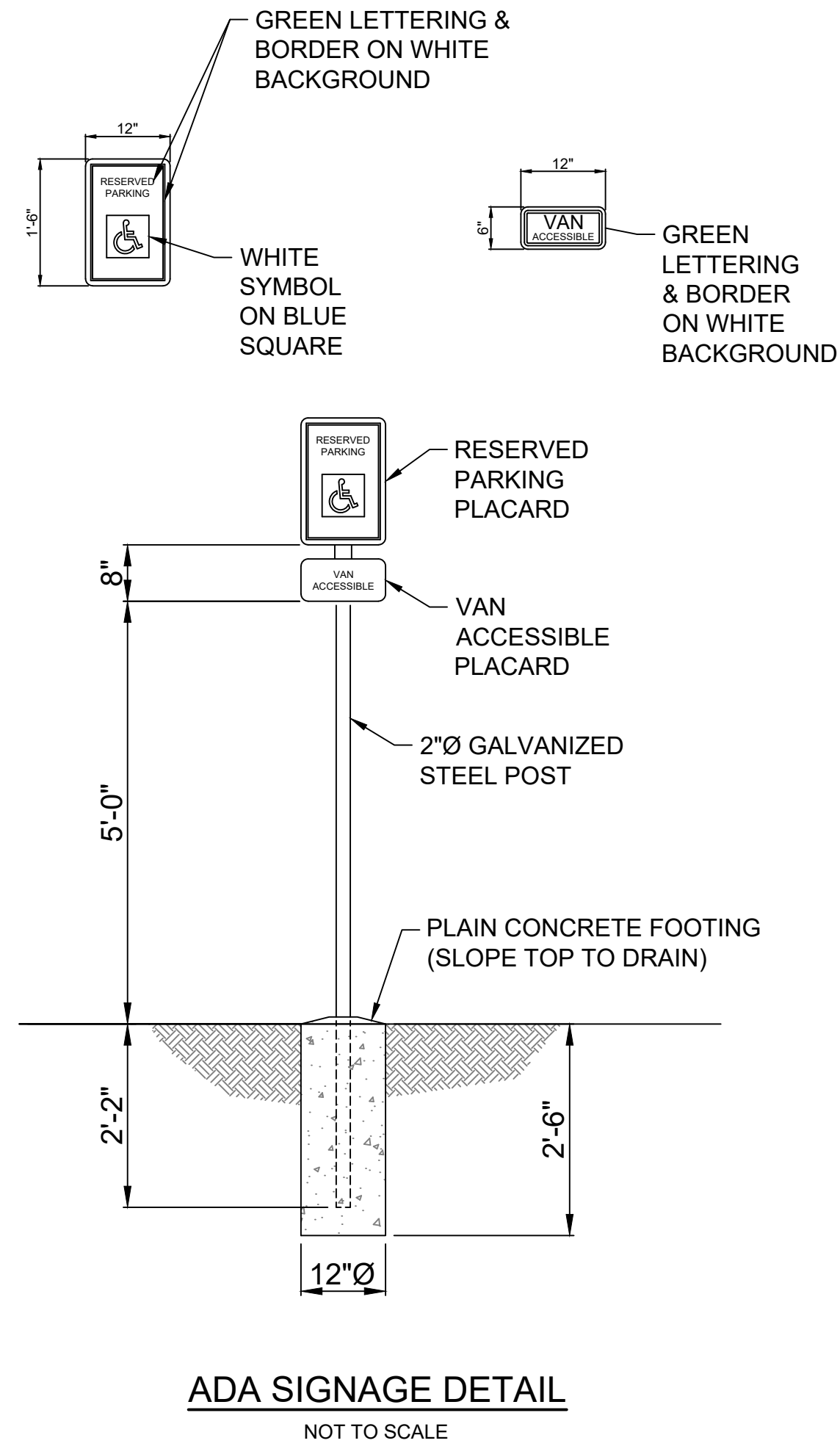
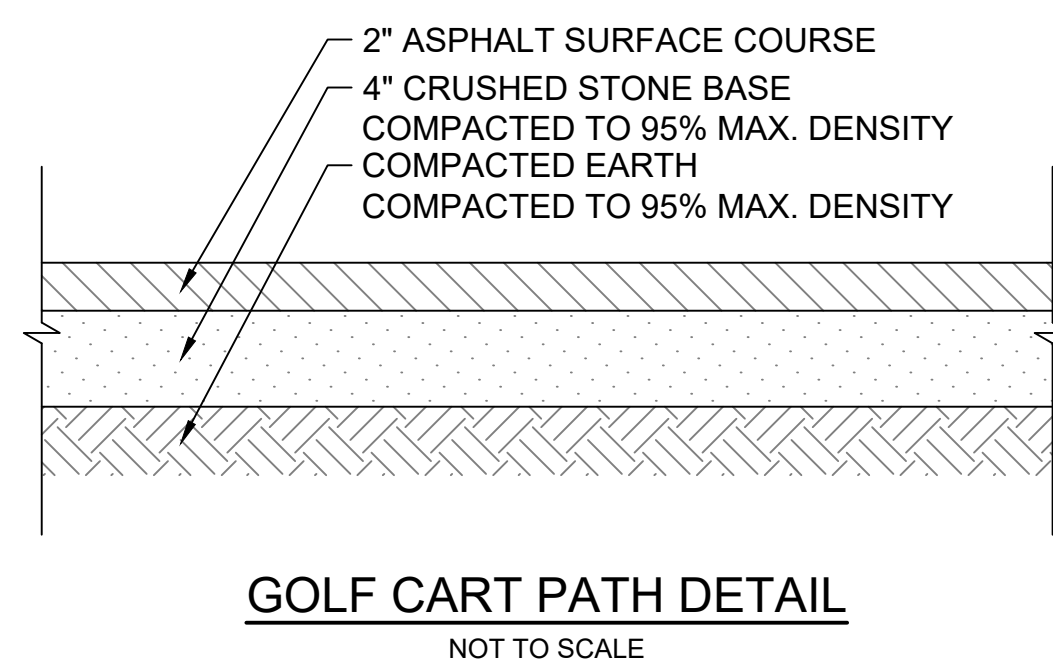
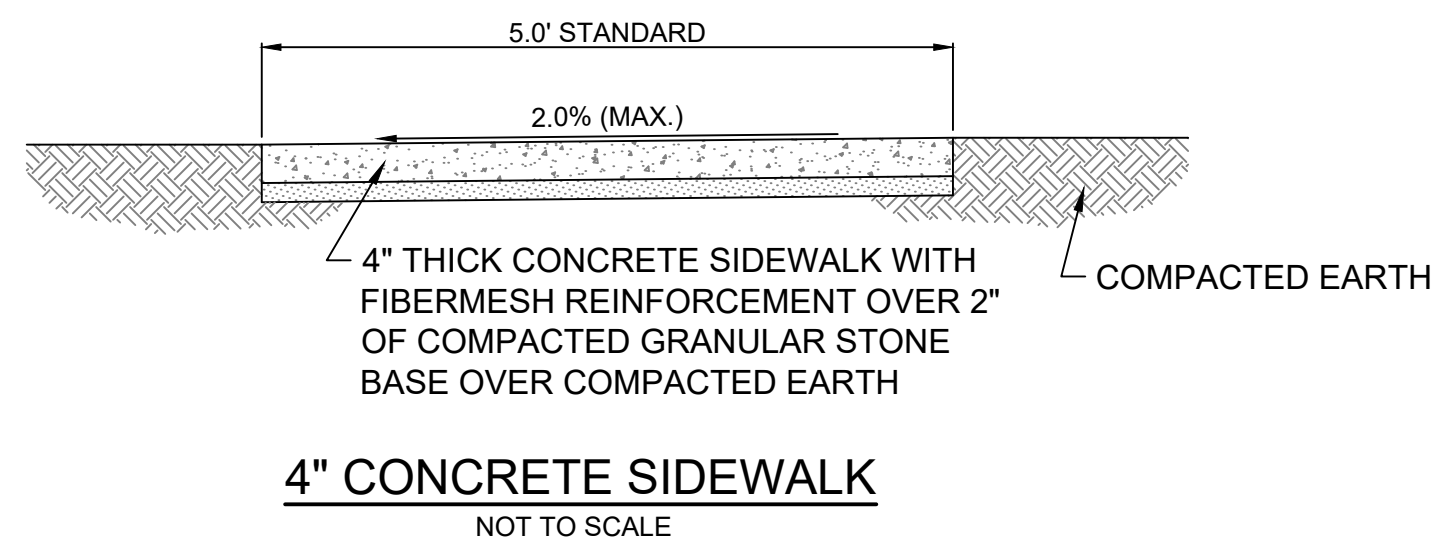
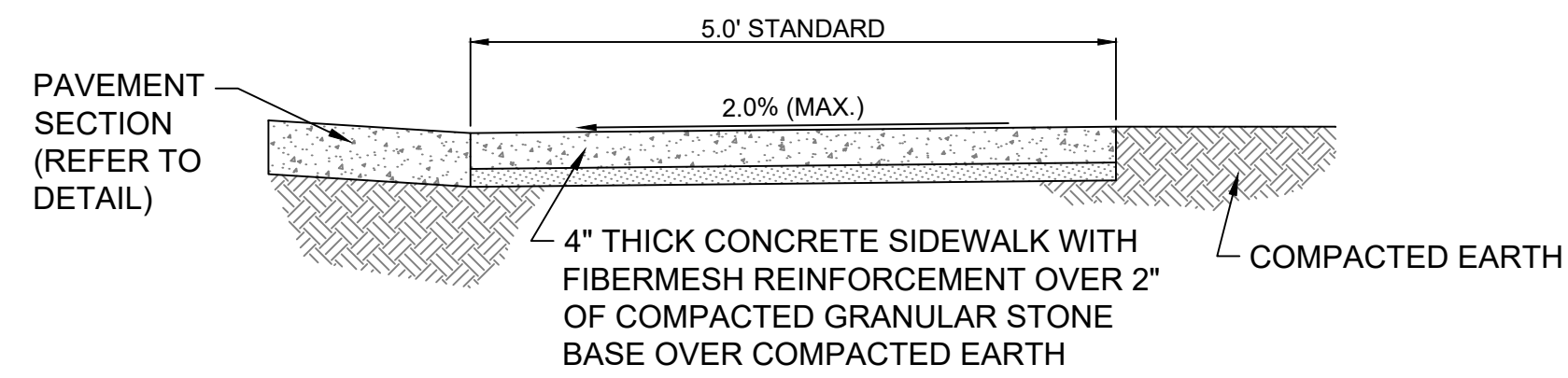
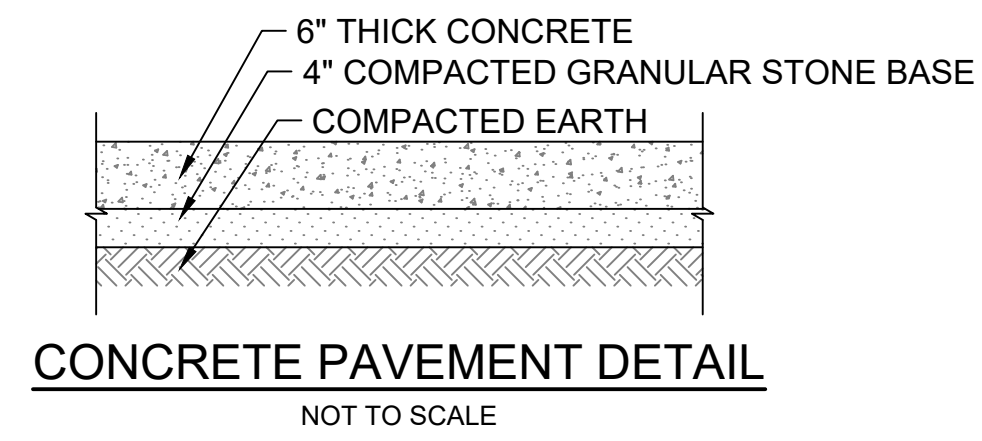
C-4.15
 OWL LOOP PROFILE



PROJECT NUMBER:
 230062
 DATE:
 08/28/2025

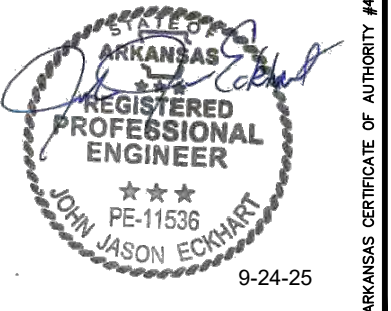
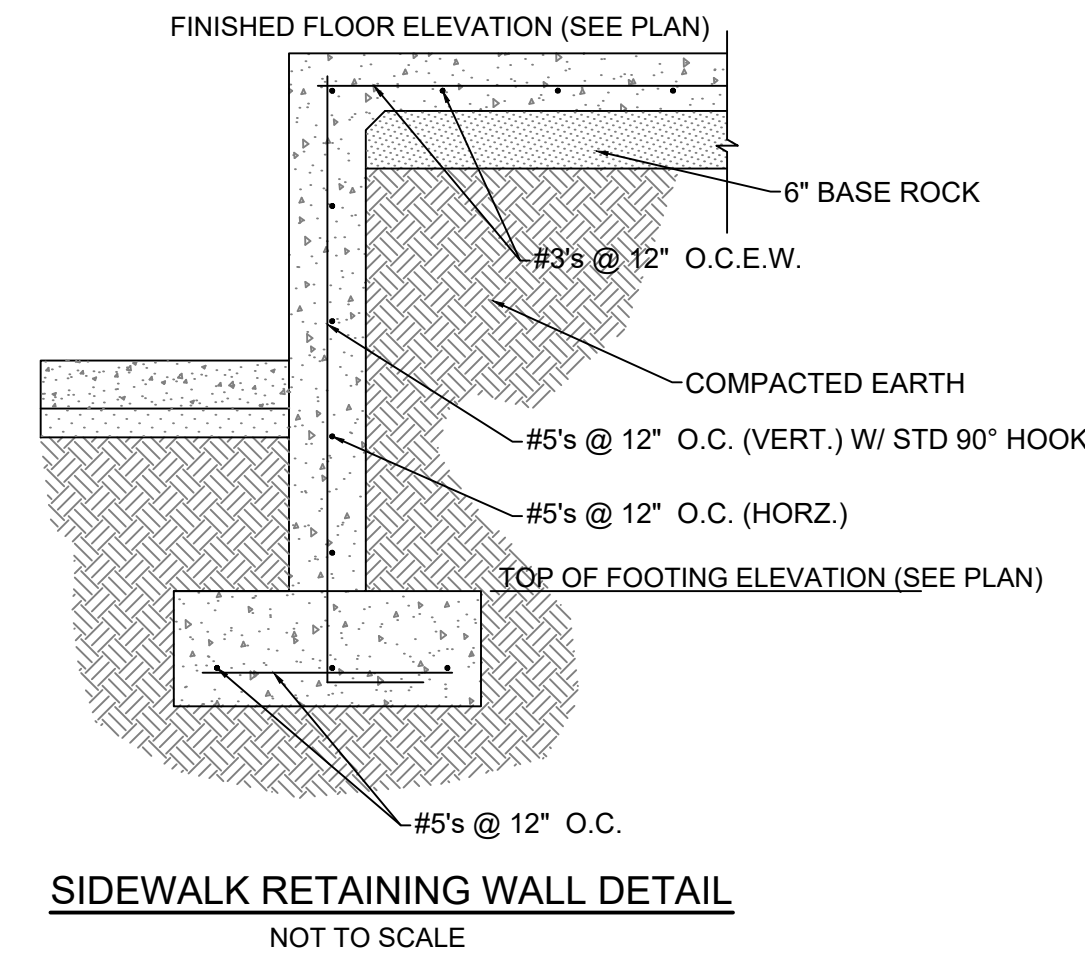
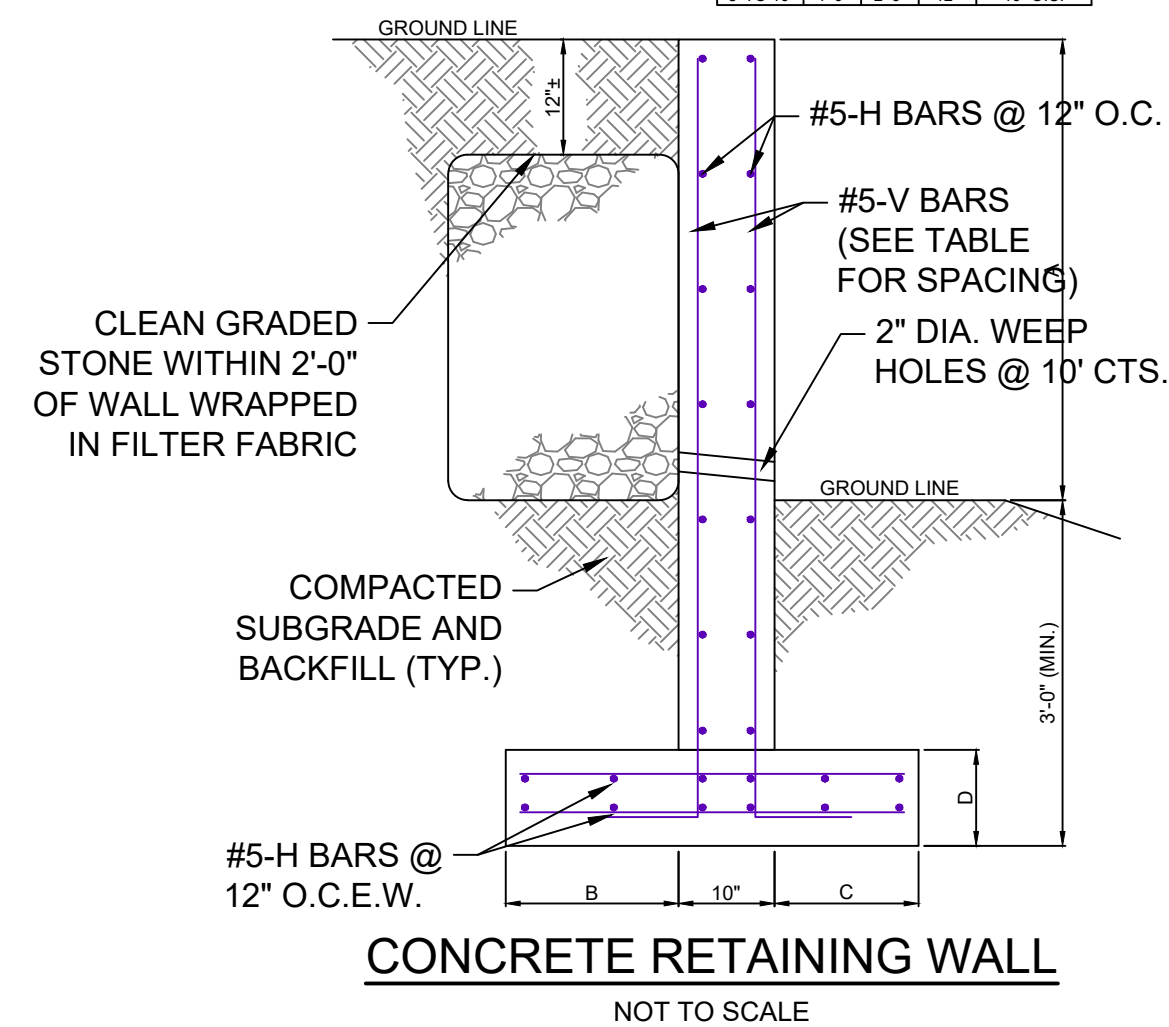


NOTE:
PAVEMENT SHALL BE CONSTRUCTED WITH LONGITUDINAL CRACK CONTROL JOINT IN CENTER. JOINT SHALL BE 1/2" DEEP AND MAY BE CONSTRUCTED BY EITHER SAWING OR TOOLING.



- NOTES:**
1. RETAINING WALL DETAIL TYP. FOR PARKING LOT WALLS. SEE SITE PLAN FOR LOCATIONS.
 2. LENGTH AND PLACEMENT OF ALL BARS SHALL PROVIDE FOR 1-1/2" CLEAR TO FORMED OR FINISHED EDGE OF WALL OR FOOTING AND 3" CLEAR AT BOTTOM OF FOOTING
 3. WEEP HOLES, FILTER FABRIC WRAP & CLEAN STONE SHALL BE CONSIDERED INCIDENTAL TO THE ITEM " CONCRETE RETAINING WALL".

A	B	C	D	#5-V BARS
UP TO 4'	1'-0"	1'-3"	10'	12' O.C.
4' TO 6'	2'-0"	1'-8"	10'	12' O.C.
6' TO 8'	2'-0"	2'-0"	10'	18' O.C.
8' TO 10'	2'-0"	2'-0"	12'	18' O.C.



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

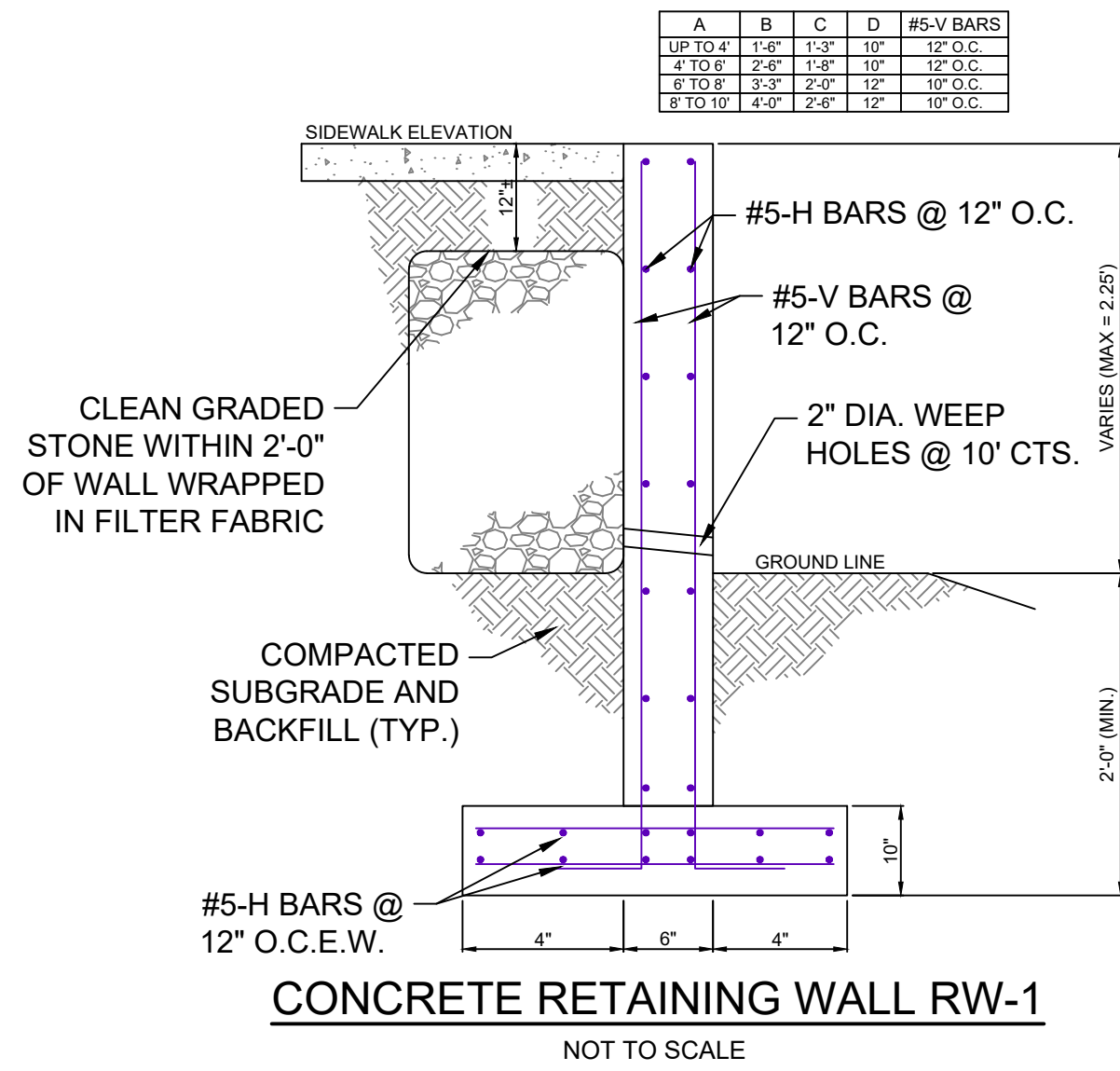
C-5.0
DETAILS

PROJECT NUMBER:
230082
DATE:
08/28/2025



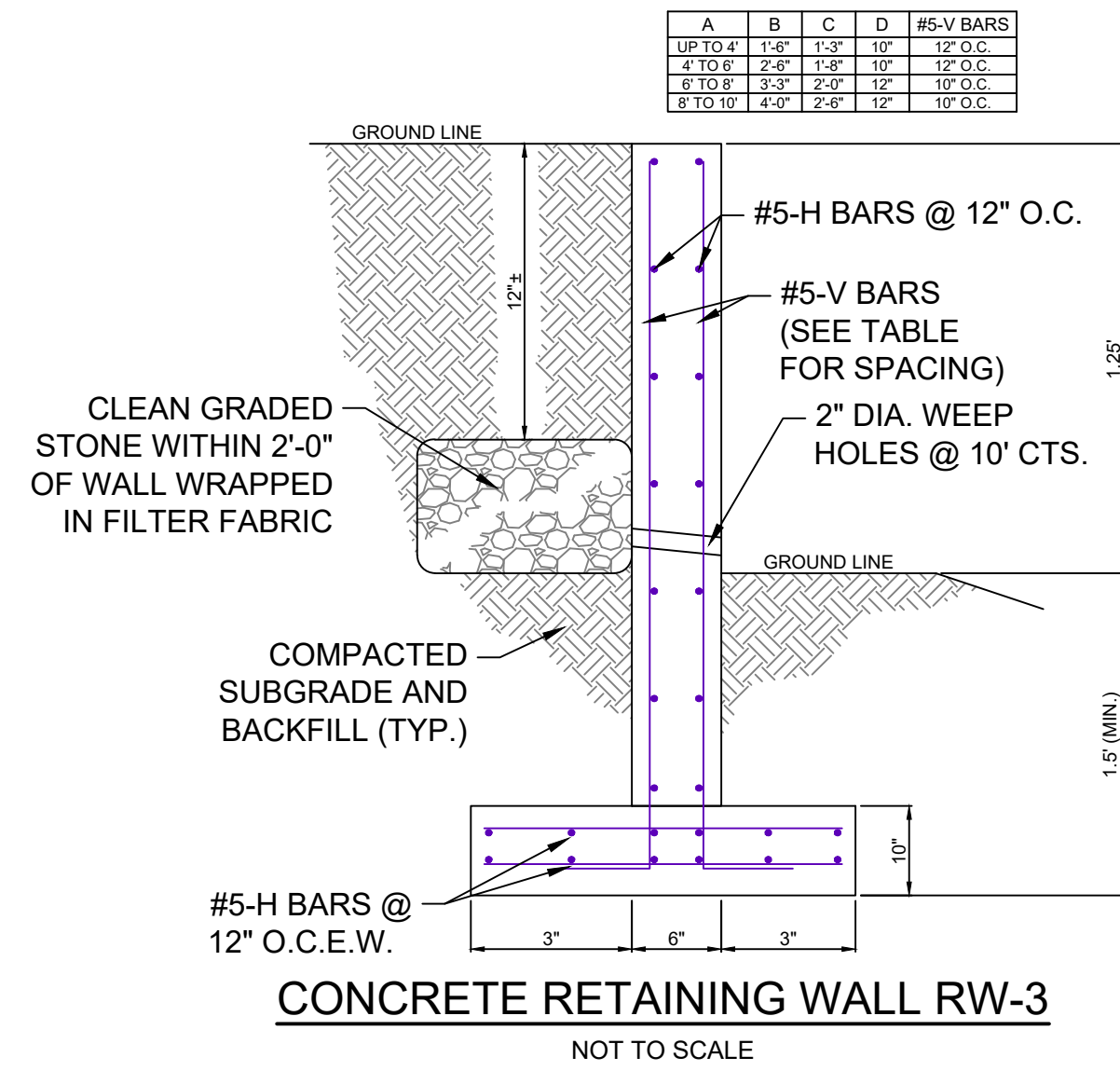
PHONE: 417-800-2500
1221 OAK ST.
CARTHAGE, MO 64636

- NOTES:**
- SEE SITE PLAN FOR LOCATION.
 - LENGTH AND PLACEMENT OF ALL BARS SHALL PROVIDE FOR 1-1/2" CLEAR TO FORMED OR FINISHED EDGE OF WALL OR FOOTING AND 3" CLEAR AT BOTTOM OF FOOTING
 - WEEP HOLES, FILTER FABRIC WRAP & CLEAN STONE SHALL BE CONSIDERED INCIDENTAL TO THE ITEM " CONCRETE RETAINING WALL".

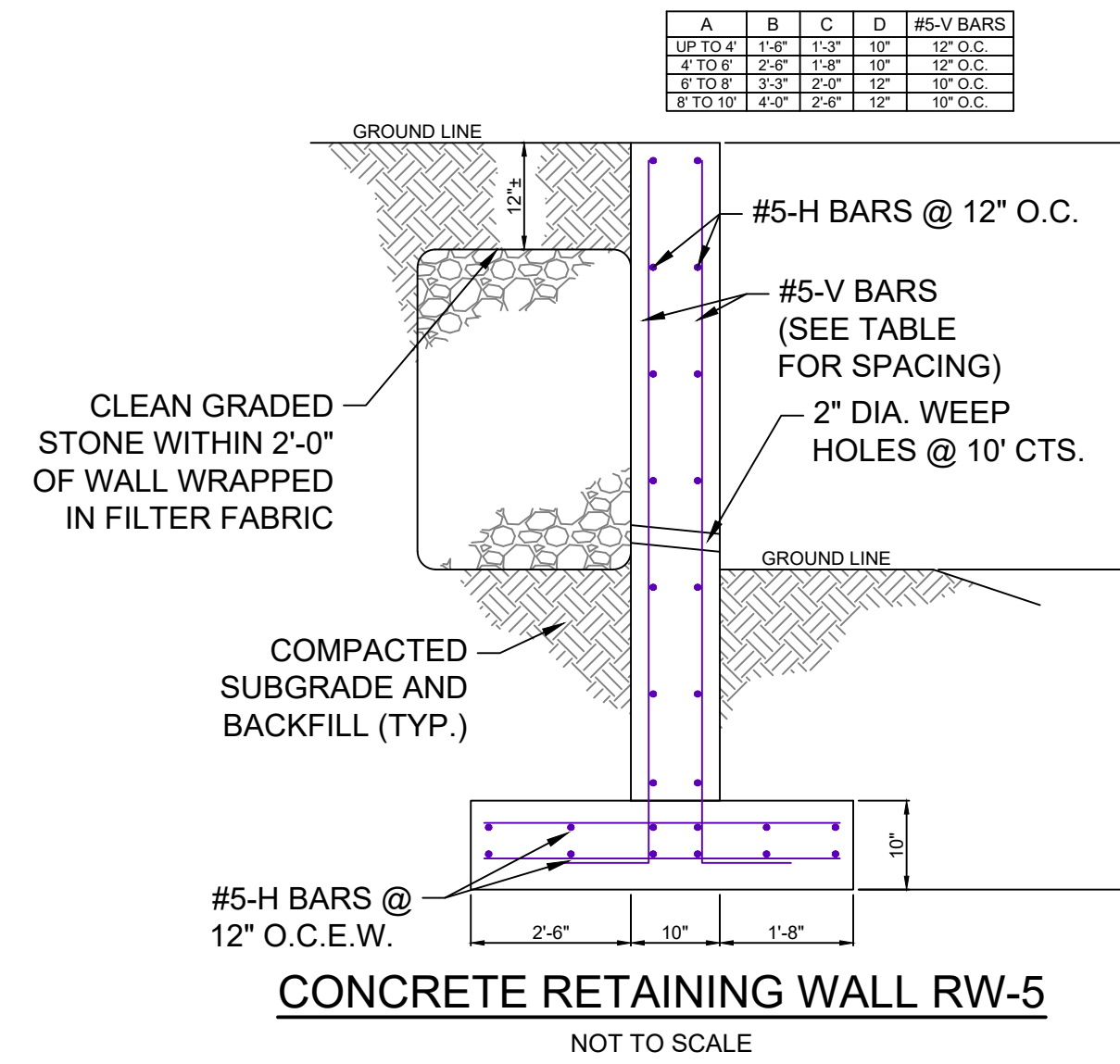


*ALSO APPLICABLE TO SECTIONS RW-2, RW-7, RW-8, DRW-1, DRW-3, DRW-4, DRW-5, DRW-8, DRW-12, DRW-13, DRW-14, DRW-15

- NOTES:**
- SEE SITE PLAN FOR LOCATION.
 - LENGTH AND PLACEMENT OF ALL BARS SHALL PROVIDE FOR 1-1/2" CLEAR TO FORMED OR FINISHED EDGE OF WALL OR FOOTING AND 3" CLEAR AT BOTTOM OF FOOTING
 - WEEP HOLES, FILTER FABRIC WRAP & CLEAN STONE SHALL BE CONSIDERED INCIDENTAL TO THE ITEM " CONCRETE RETAINING WALL".



- NOTES:**
- SEE SITE PLAN FOR LOCATION.
 - LENGTH AND PLACEMENT OF ALL BARS SHALL PROVIDE FOR 1-1/2" CLEAR TO FORMED OR FINISHED EDGE OF WALL OR FOOTING AND 3" CLEAR AT BOTTOM OF FOOTING
 - WEEP HOLES, FILTER FABRIC WRAP & CLEAN STONE SHALL BE CONSIDERED INCIDENTAL TO THE ITEM " CONCRETE RETAINING WALL".



LOAD CONDITION A | NO LIVE LOAD SURCHARGE, NO BACK SLOPE, NO TOE SLOPE

9 BLOCK HIGH SECTION
(9) 28 in (710 mm) Blocks

PRELIMINARY
Professional Engineering Design
Required for Construction

Geogrid shall be 12 in (305 mm) wide strips of Miraf geogrid, type as noted. Geogrid shall be factory cut and certified for width and strength by Solmax Miraf.

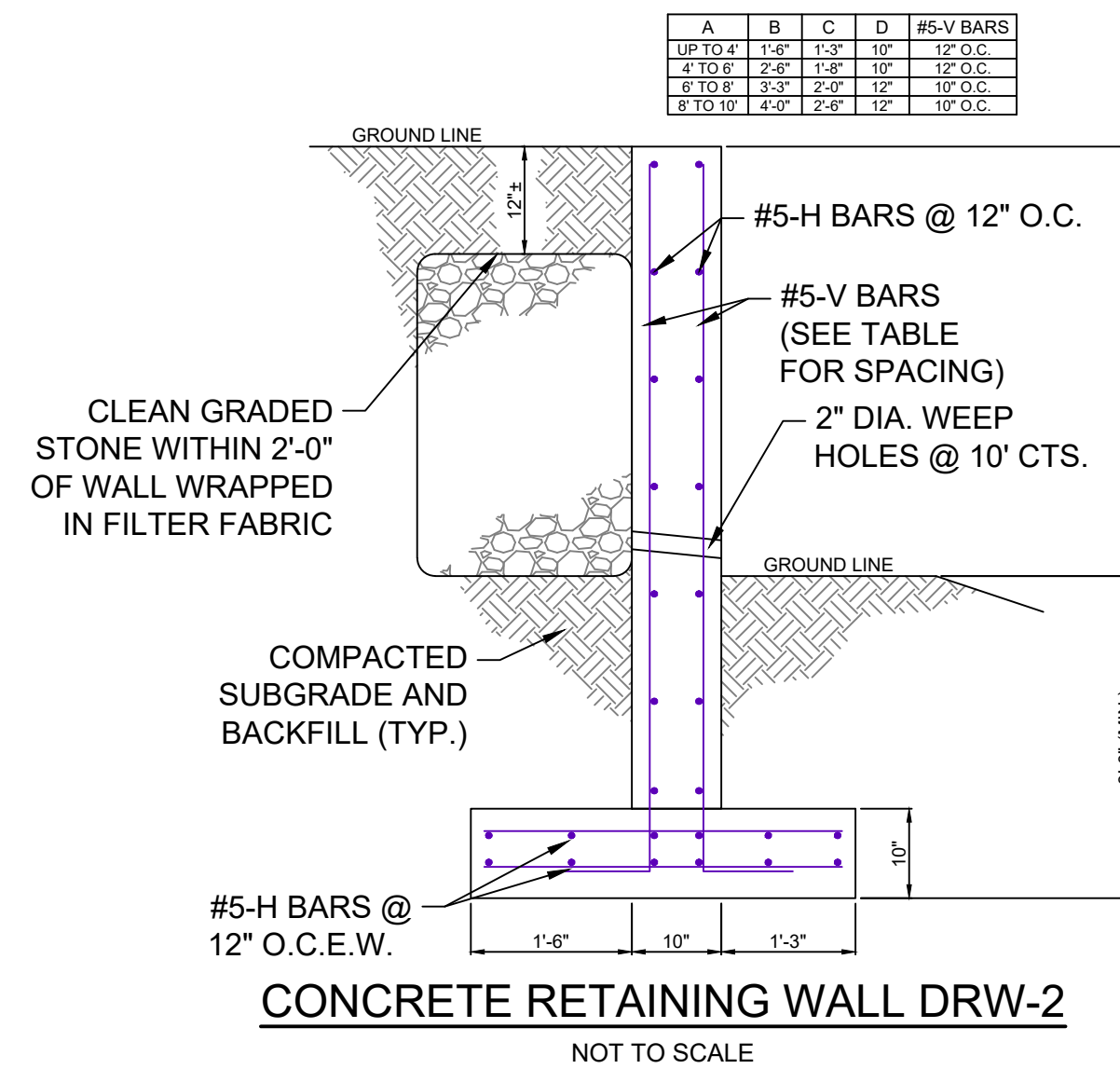
Geogrid cut length:
= 9 ft + 9 ft + 3 ft
= 21 ft (6.40 m)

Geogrid Rolls Required per Wall Length	Type	Rolls / linear ft	Rolls / linear m
5XT		± 0.26	± 0.85

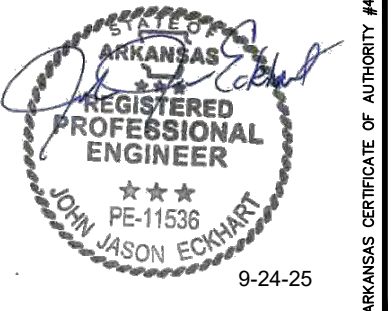
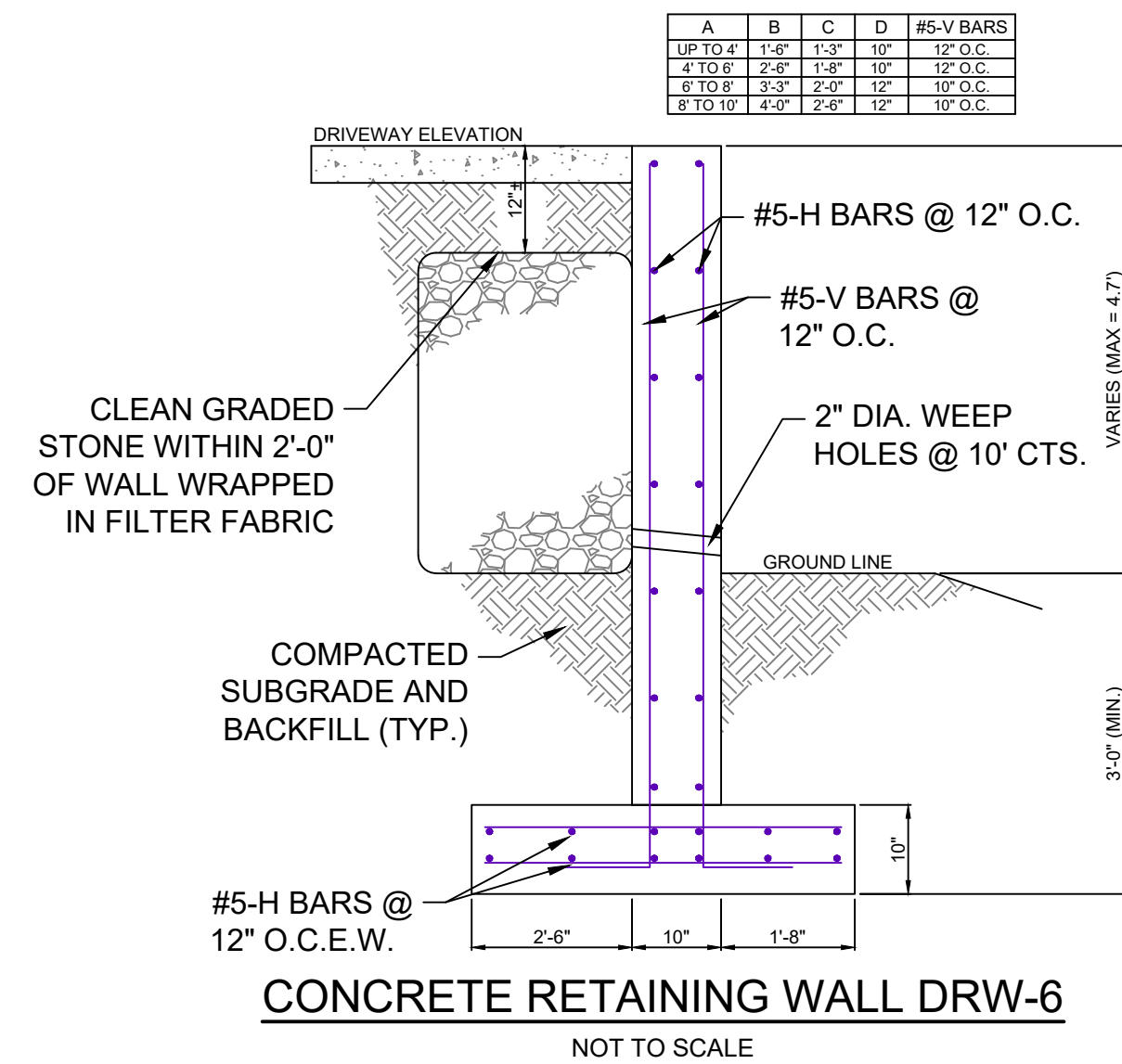
A	B	C	D	#5-V BARS
UP TO 4'	1'-6"	1'-3"	1'-0"	12" O.C.
4' TO 6'	2'-6"	1'-8"	1'-0"	12" O.C.
6' TO 8'	3'-3"	2'-0"	1'-0"	12" O.C.
8' TO 10'	4'-3"	2'-8"	1'-0"	12" O.C.

CONCRETE RETAINING WALL RW-2
NOT TO SCALE

- NOTES:**
- SEE SITE PLAN FOR LOCATION.
 - LENGTH AND PLACEMENT OF ALL BARS SHALL PROVIDE FOR 1-1/2" CLEAR TO FORMED OR FINISHED EDGE OF WALL OR FOOTING AND 3" CLEAR AT BOTTOM OF FOOTING
 - WEEP HOLES, FILTER FABRIC WRAP & CLEAN STONE SHALL BE CONSIDERED INCIDENTAL TO THE ITEM " CONCRETE RETAINING WALL".



- NOTES:**
- SEE SITE PLAN FOR LOCATION.
 - LENGTH AND PLACEMENT OF ALL BARS SHALL PROVIDE FOR 1-1/2" CLEAR TO FORMED OR FINISHED EDGE OF WALL OR FOOTING AND 3" CLEAR AT BOTTOM OF FOOTING
 - WEEP HOLES, FILTER FABRIC WRAP & CLEAN STONE SHALL BE CONSIDERED INCIDENTAL TO THE ITEM " CONCRETE RETAINING WALL".



NO.	REVISIONS	DESCRIPTION	BY	DATE

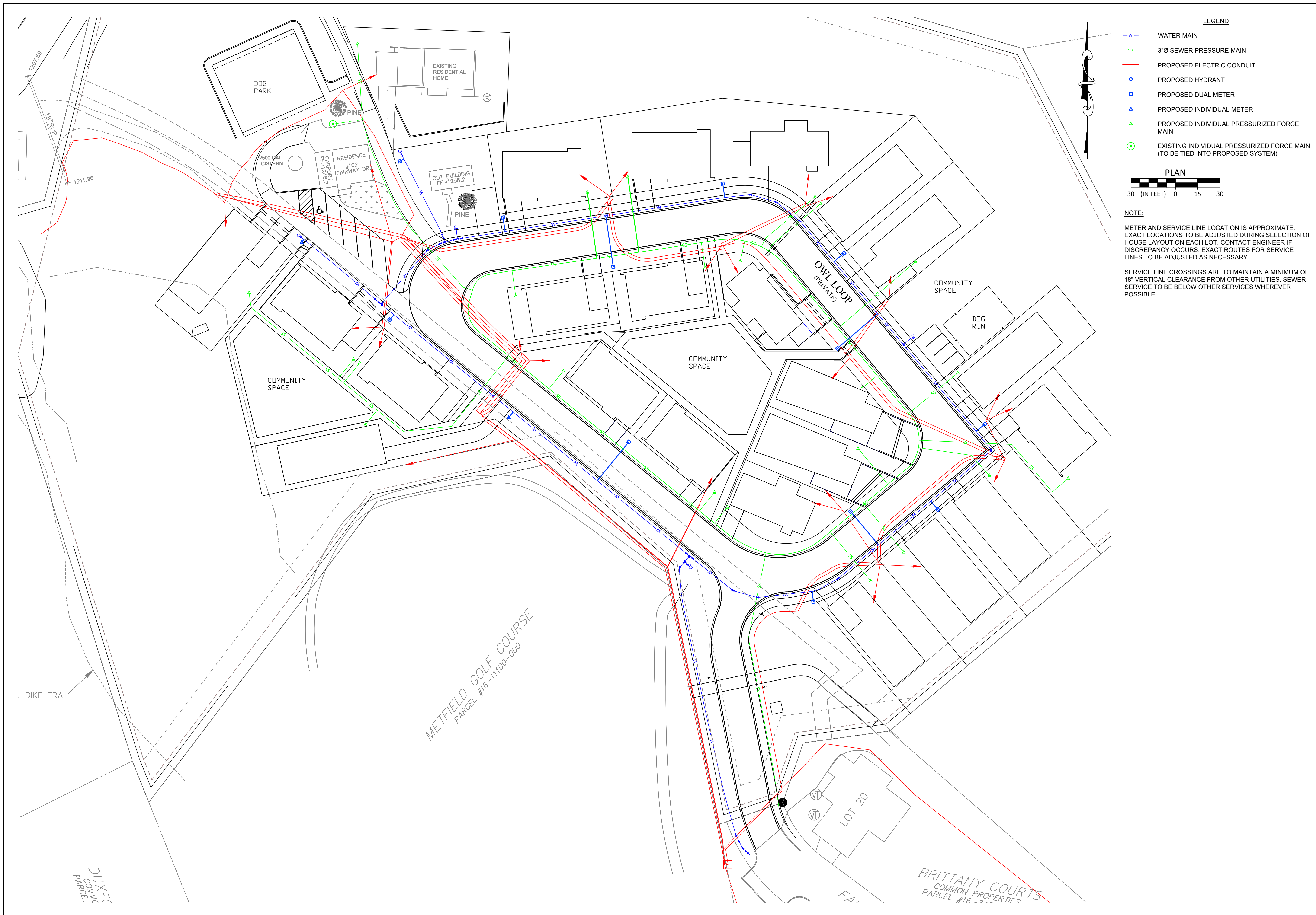
OWL COMMONS

102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-5.1
RETAINING WALL
DETAILS

PROJECT NUMBER:
230082
DATE:
08/28/2025



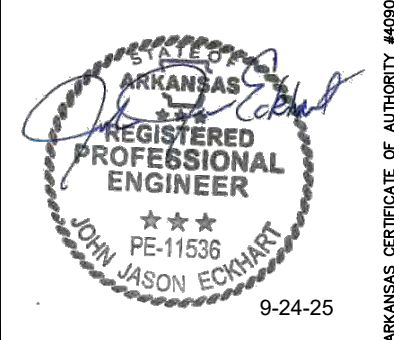


LEGEND

- W- WATER MAIN
- SS- 3"Ø SEWER PRESSURE MAIN
- - - PROPOSED ELECTRIC CONDUIT
- PROPOSED HYDRANT
- PROPOSED DUAL METER
- ▲ PROPOSED INDIVIDUAL METER
- ▲ PROPOSED INDIVIDUAL PRESSURIZED FORCE MAIN
- EXISTING INDIVIDUAL PRESSURIZED FORCE MAIN (TO BE TIED INTO PROPOSED SYSTEM)



NOTE:
METER AND SERVICE LINE LOCATION IS APPROXIMATE. EXACT LOCATIONS TO BE ADJUSTED DURING SELECTION OF HOUSE LAYOUT ON EACH LOT. CONTACT ENGINEER IF DISCREPANCY OCCURS. EXACT ROUTES FOR SERVICE LINES TO BE ADJUSTED AS NECESSARY.
SERVICE LINE CROSSINGS ARE TO MAINTAIN A MINIMUM OF 18" VERTICAL CLEARANCE FROM OTHER UTILITIES. SEWER SERVICE TO BE BELOW OTHER SERVICES WHEREVER POSSIBLE.



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

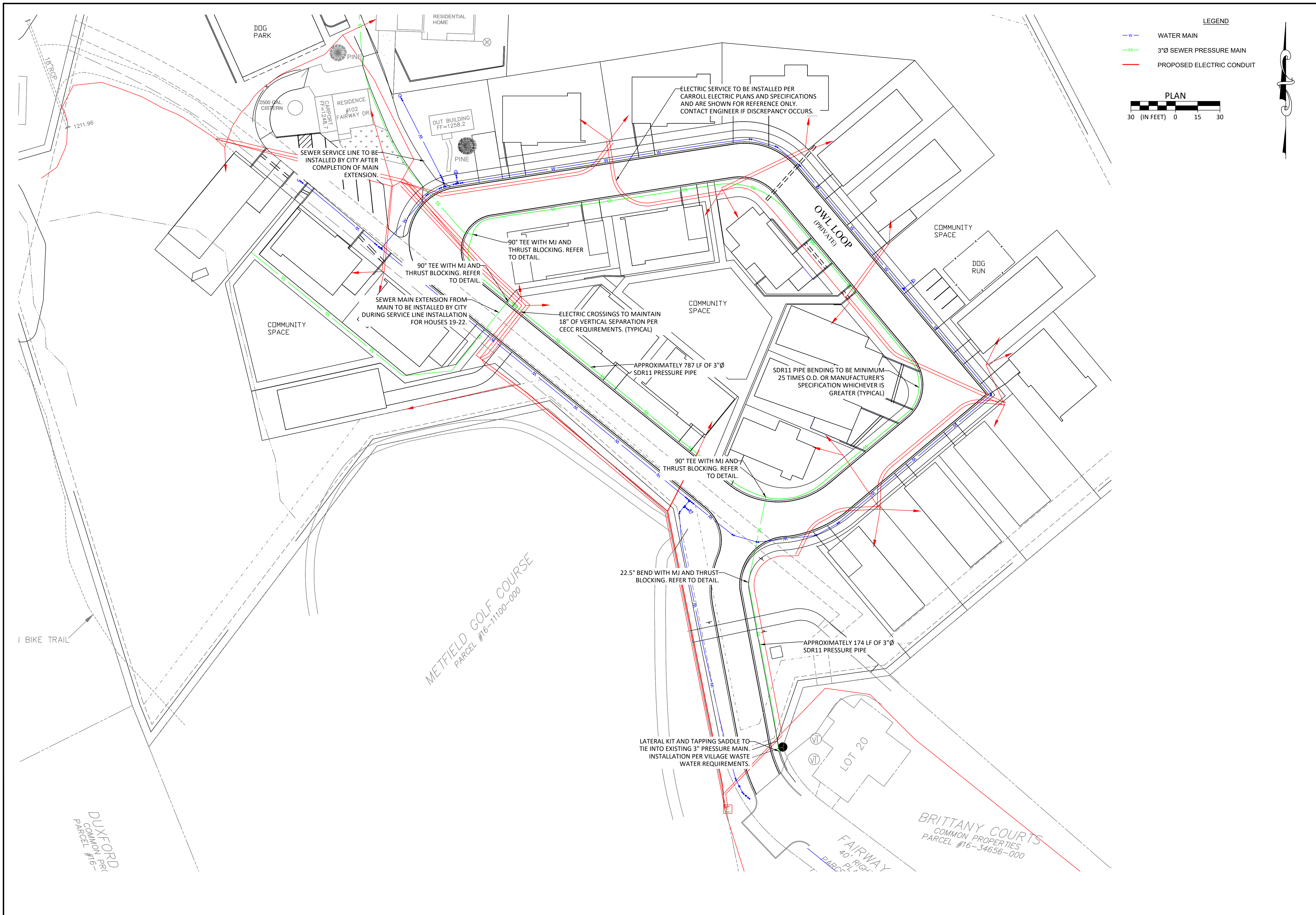
102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-6.0
UTILITY OVERVIEW &
SERVICE LINES

PROJECT NUMBER:
230082
DATE:
08/28/2025



PHONE: 417-600-2500
1221 OAK ST.
CAPRICE, MO 64636



LEGEND

- WATER MAIN
- 3"Ø SEWER PRESSURE MAIN
- PROPOSED ELECTRIC CONDUIT

PLAN

30 (IN FEET) 0 15 30



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

C-7.0

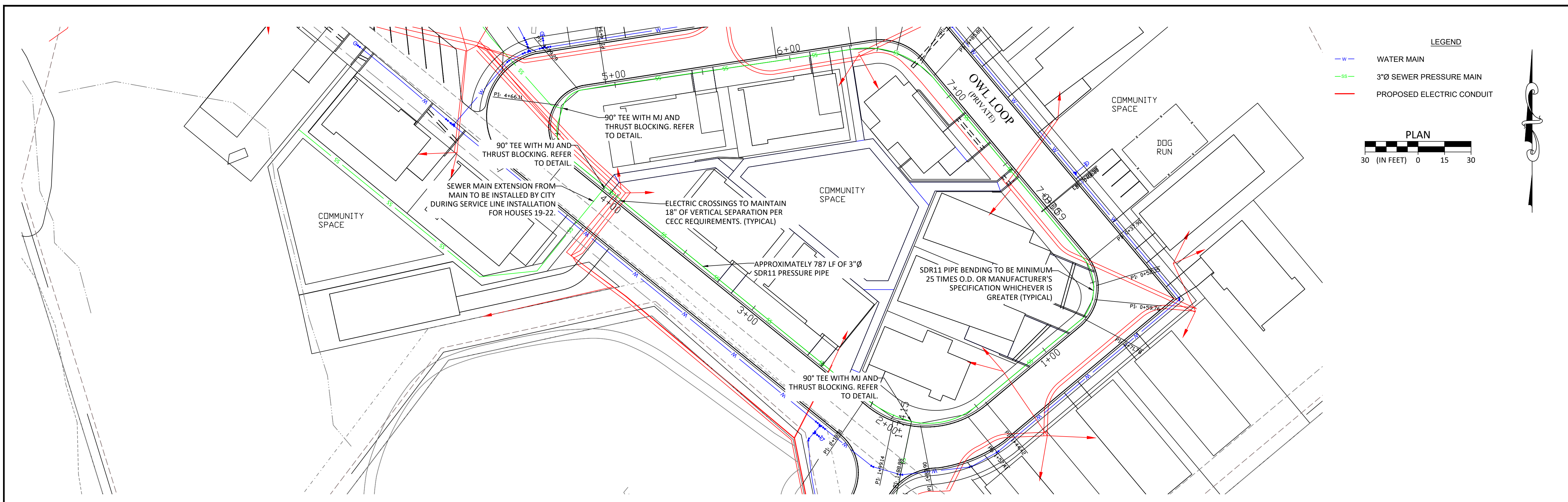
SEWER MAIN LAYOUT

PROJECT NUMBER:
230082
DATE:
08/28/2025

102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714



PHONE: 417-600-2500
1221 OAK ST.
CAPTRAGE, MO 64636

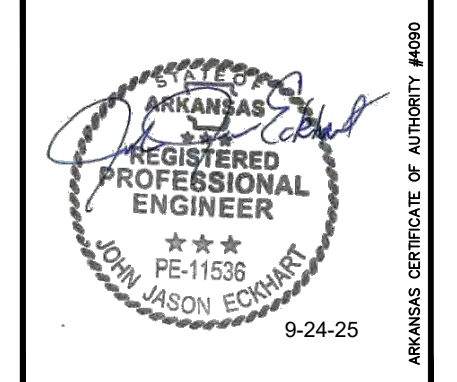


LEGEND

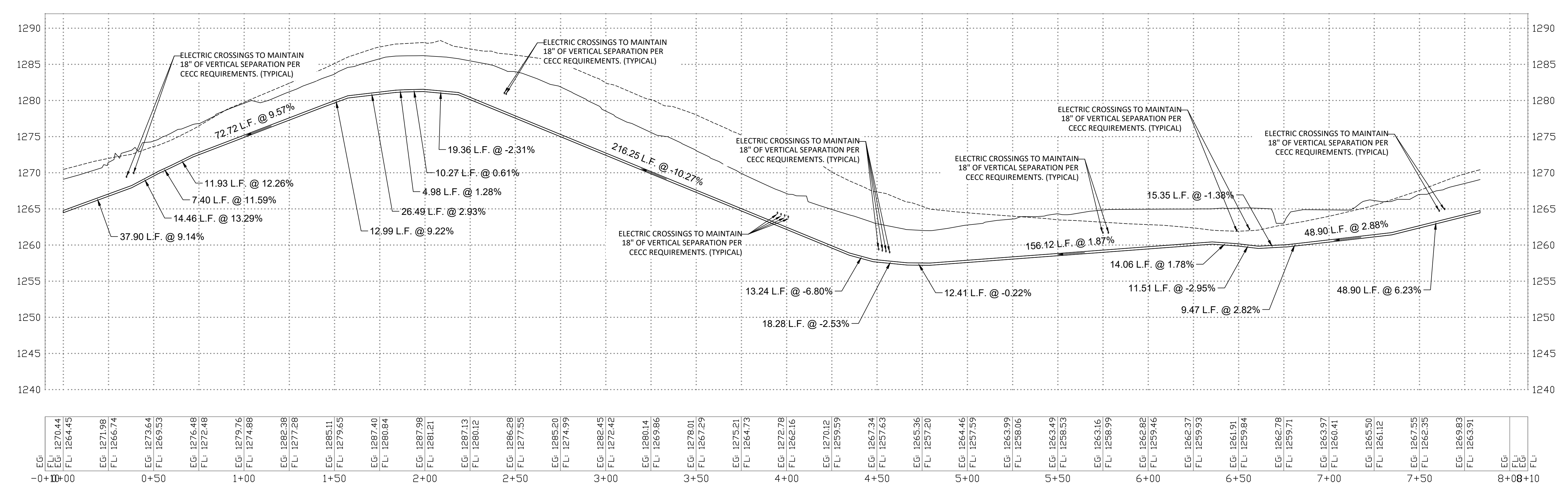
- W WATER MAIN
- SS 3"Ø SEWER PRESSURE MAIN
- PROPOSED ELECTRIC CONDUIT

PLAN

30 (IN FEET) 0 15 30



NO.	REVISIONS	DESCRIPTION	DATE	BY



OWL COMMONS

102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

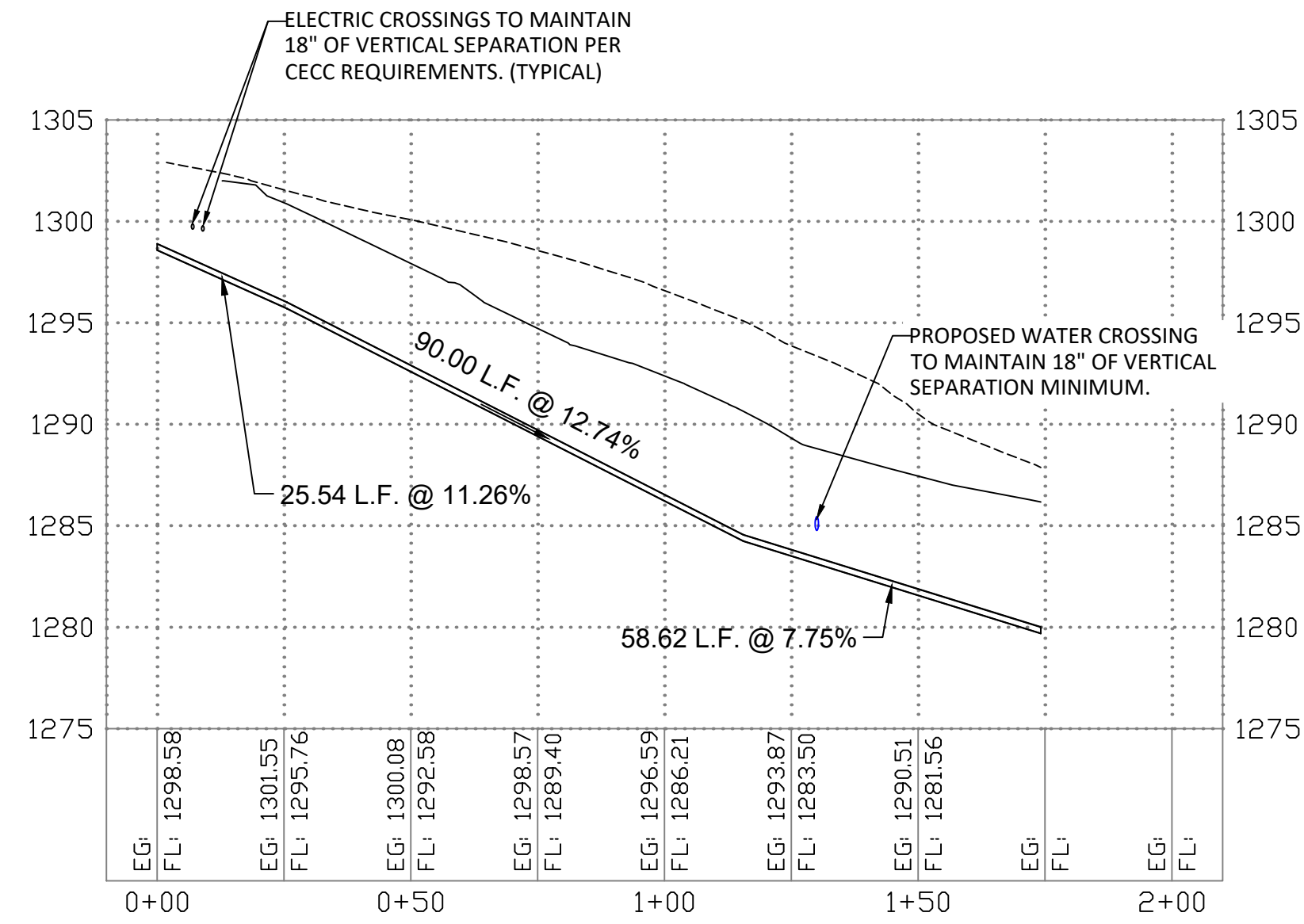
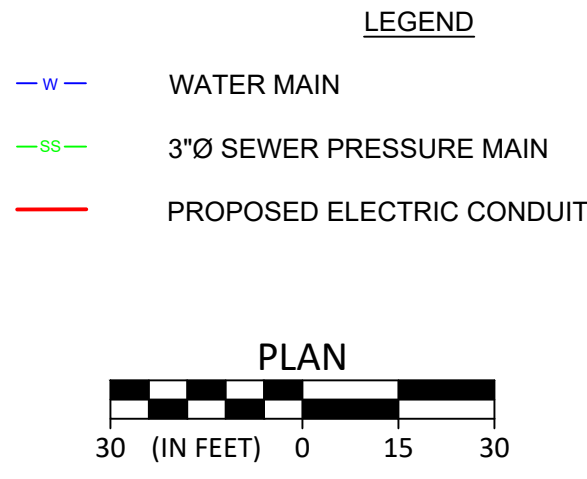
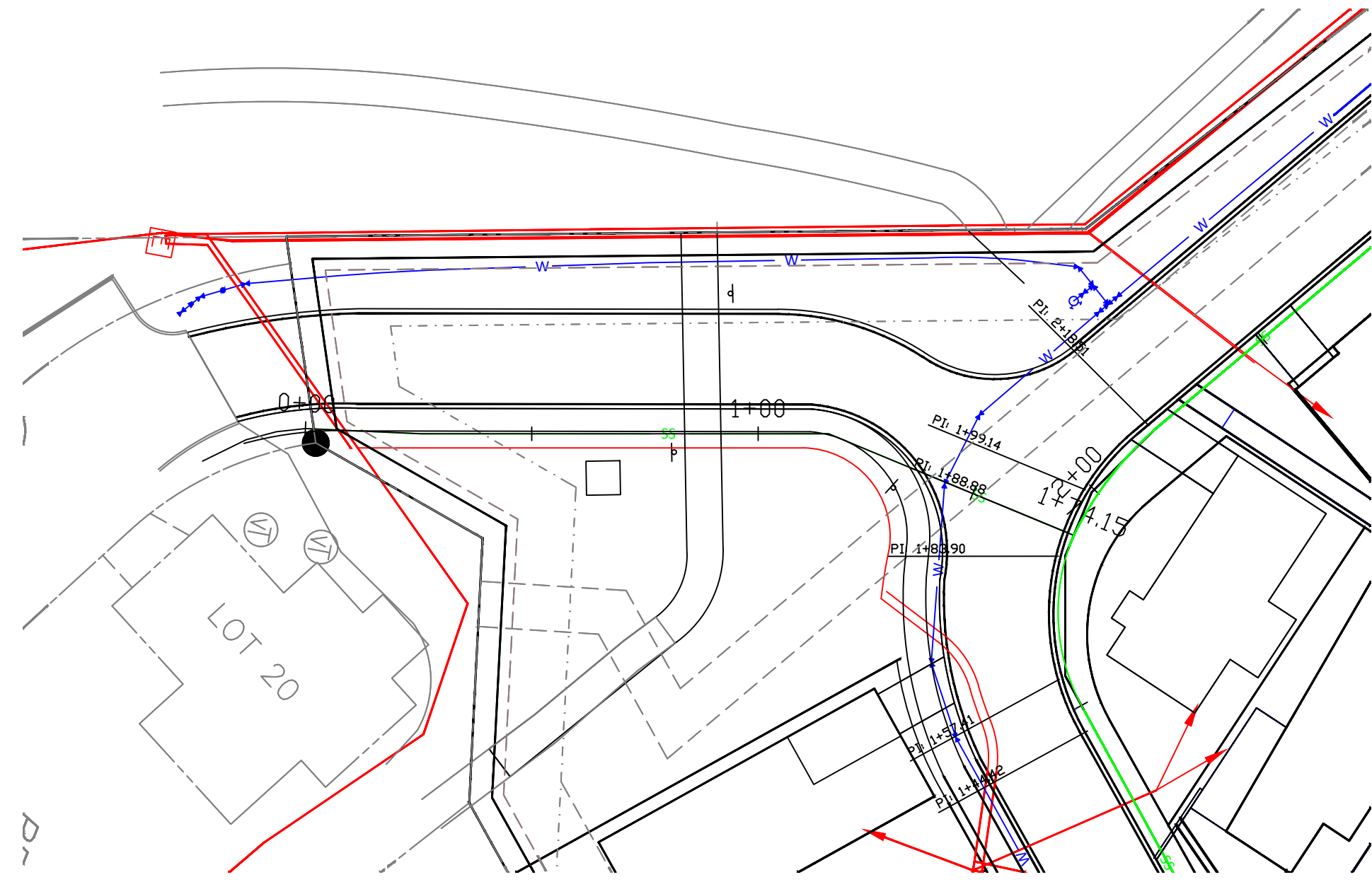
C-7.1

SEWER MAIN PROFILE

PROJECT NUMBER: 230082
 DATE: 08/28/2025

ZANEVAN ENGINEERING

PHONE: 417-800-2500
 CARROLLTON, MO 64836



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

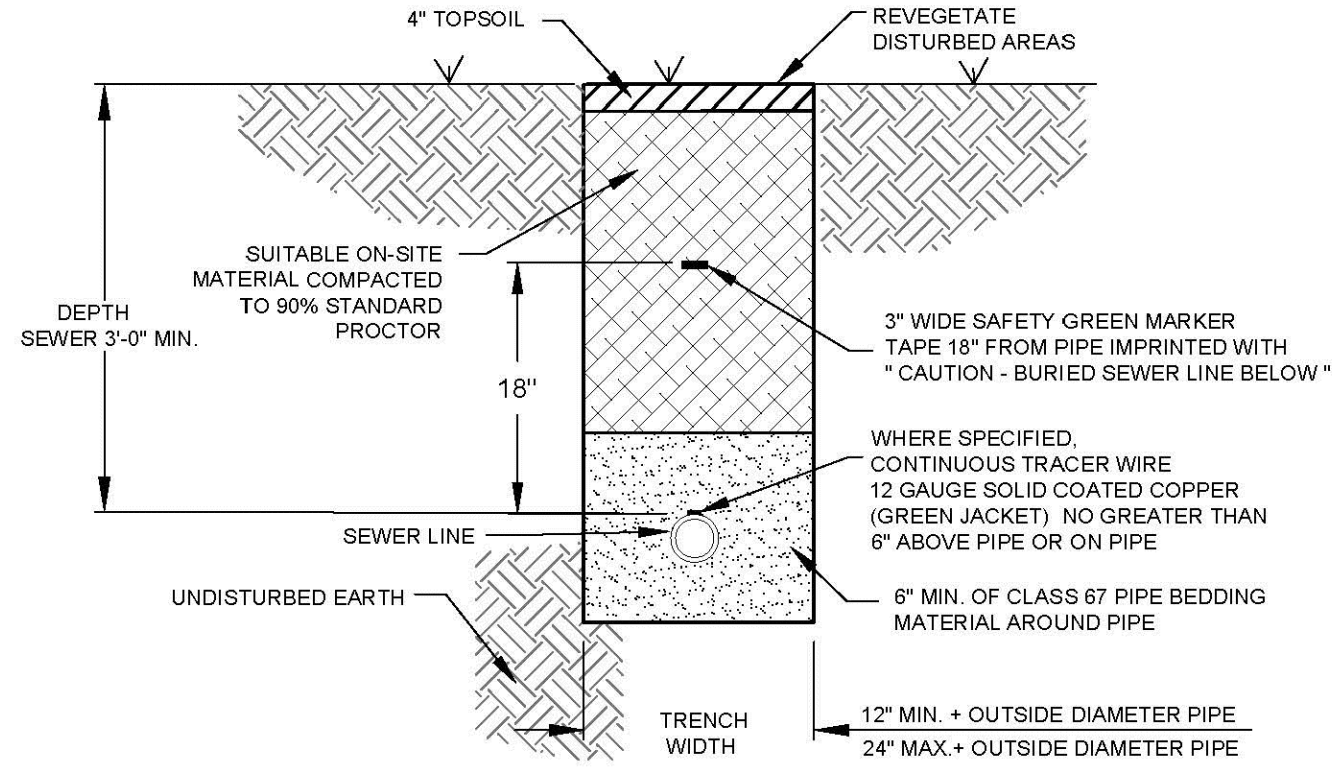
102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-7.2
SEWER MAIN PROFILE

PROJECT NUMBER:
230082
DATE:
08/28/2025



PHONE: 417-600-2500
1221 OAK ST.
CAPRICE, MO 64636

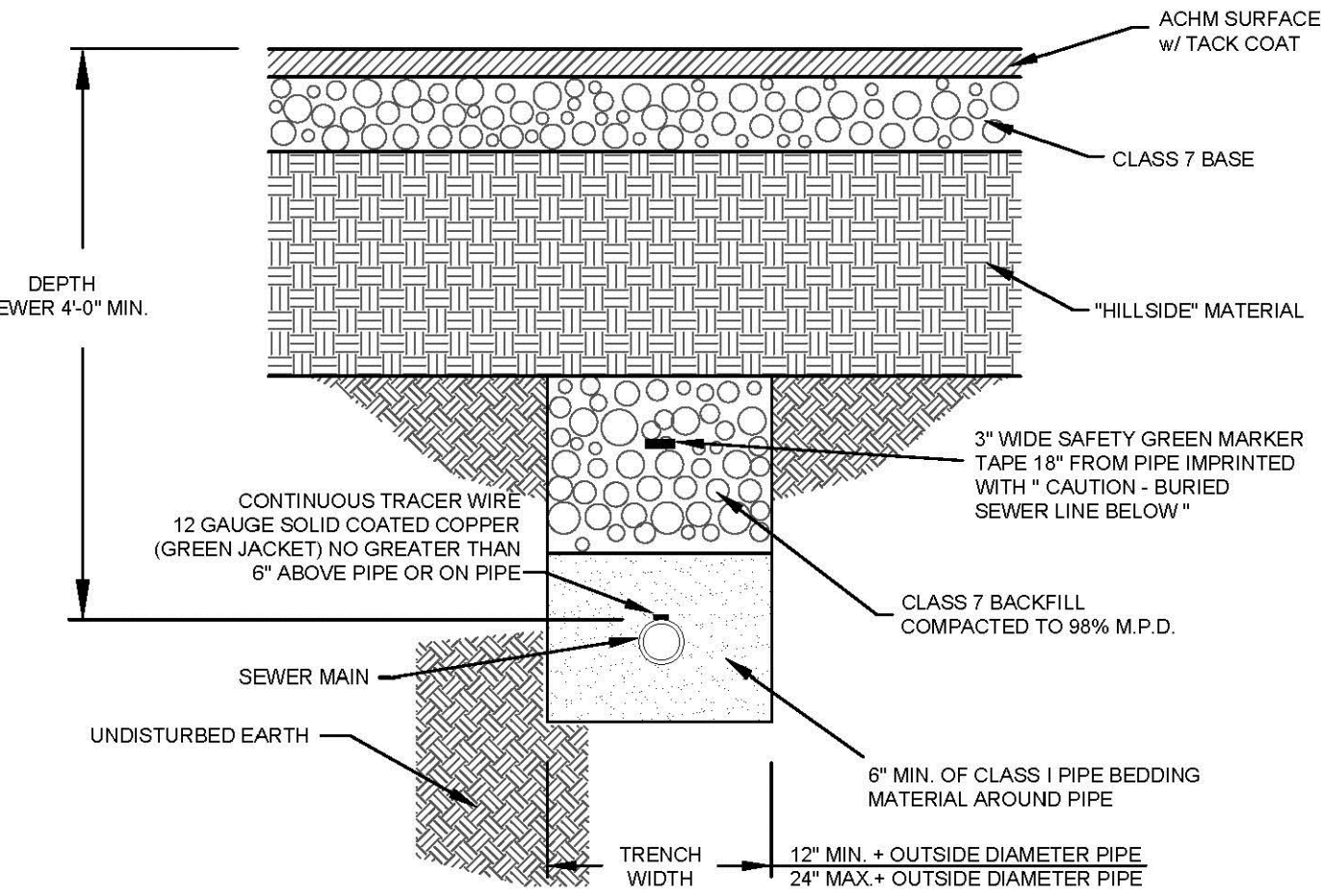


NOTES:

1. THE CONTRACTOR SHALL PROVIDE ALL ITEMS NECESSARY TO CONNECT WITH ANY PART OF THE EXISTING SEWER SYSTEM THAT WILL REMAIN IN ORDER TO ESTABLISH A SATISFACTORY AND ACCEPTABLE SEWER SYSTEM.
2. CONTRACTOR TO CONSTRUCT ALL TRENCH EXCAVATION IN ACCORDANCE WITH ALL OSHA REGULATIONS (29 CFR CH.XVII, SUBPART B)
3. TRENCH SHALL BE EXCAVATED BELOW GRADE REQUIRED TO PROVIDE A MINIMUM 36" OF PIPE COVER.
4. PIPE BEDDING SHALL HAVE A MAXIMUM PARTICLE SIZE OF 3/4"

SEWER TRENCH
(NON-PAVED AREA)

	CITY OF FAYETTEVILLE ARKANSAS	ENGINEERING DIVISION 113 W. MOUNTAIN STREET	TITLE: SEWER DETAILS	DATE: MAY 24, 2022
			DESCRIPTION: SEWER TRENCH (NON-PAVED AREA)	SHEET: S1



NOTES:

1. THE CONTRACTOR SHALL PROVIDE ALL ITEMS NECESSARY TO CONNECT WITH ANY PART OF THE EXISTING SEWER SYSTEM THAT WILL REMAIN IN ORDER TO ESTABLISH A SATISFACTORY AND ACCEPTABLE SEWER SYSTEM.
2. CONTRACTOR TO CONSTRUCT ALL TRENCH EXCAVATION IN ACCORDANCE WITH ALL OSHA REGULATIONS (29 CFR CH.XVII, SUBPART B)
3. TRENCH SHALL BE EXCAVATED BELOW GRADE REQUIRED TO PROVIDE A MINIMUM 36" OF PIPE COVER.
4. PIPE BEDDING SHALL HAVE A MAXIMUM PARTICLE SIZE OF 3/4"

SEWER MAIN INSTALLATION
PRIOR TO ROADWAY CONSTRUCTION

	CITY OF FAYETTEVILLE ARKANSAS	ENGINEERING DIVISION 113 W. MOUNTAIN STREET	TITLE: SEWER DETAILS	DATE: MAY 24, 2022
			DESCRIPTION: SEWER MAIN INSTALLATION PRIOR TO ROADWAY CONSTRUCTION	SHEET: S3



NO.	REVISIONS	BY	DATE

OWL COMMONS

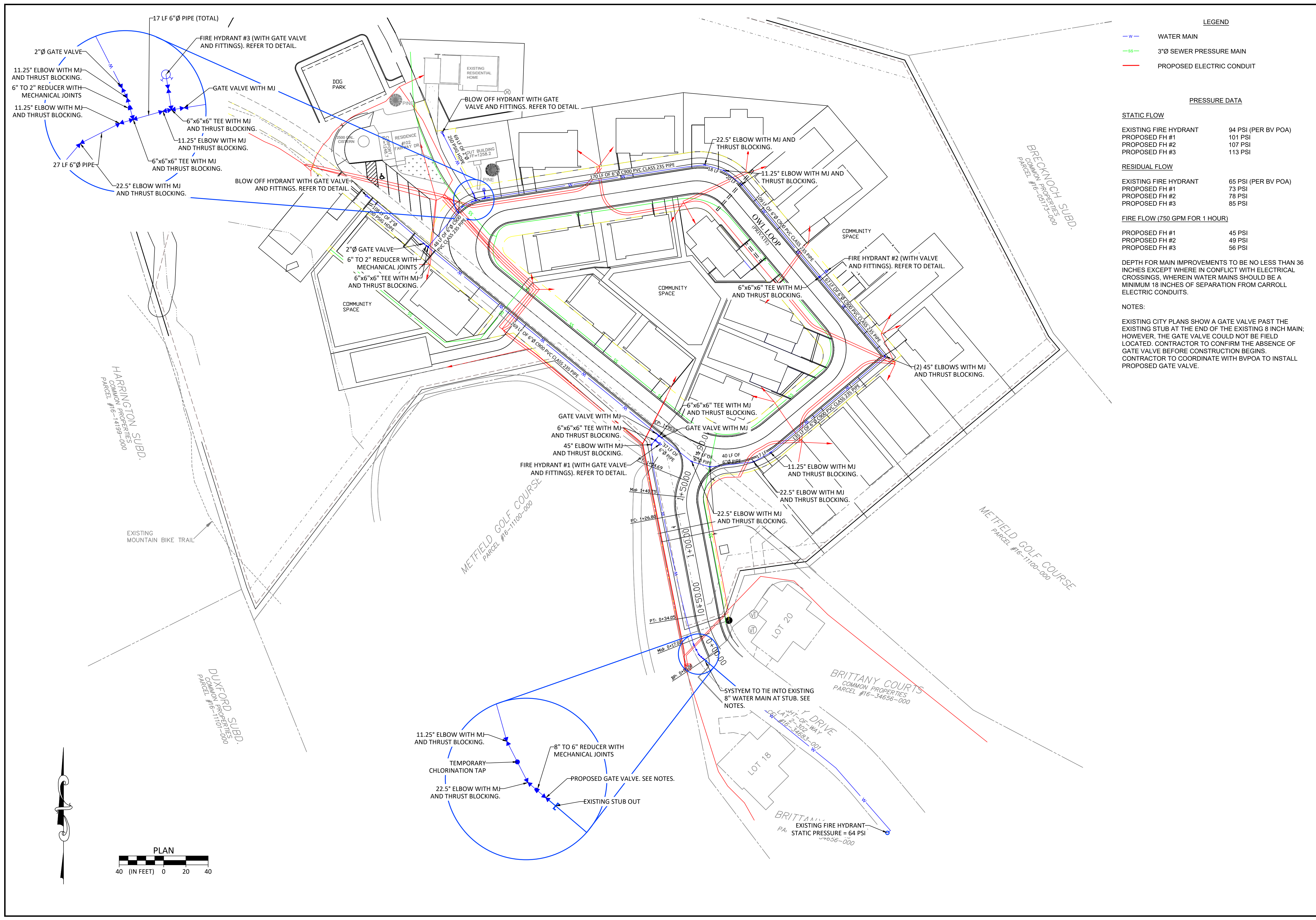
102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-7.3
SEWER DETAILS

PROJECT NUMBER:
230082
DATE:
08/28/2025



PHONE: 417-600-2500
1221 OAK ST.
CAPRICE, MO 64636



LEGEND

- W- WATER MAIN
- SS- 3"Ø SEWER PRESSURE MAIN
- E- PROPOSED ELECTRIC CONDUIT

PRESSURE DATA

STATIC FLOW

EXISTING FIRE HYDRANT	94 PSI (PER BV POA)
PROPOSED FH #1	101 PSI
PROPOSED FH #2	107 PSI
PROPOSED FH #3	113 PSI

RESIDUAL FLOW

EXISTING FIRE HYDRANT	65 PSI (PER BV POA)
PROPOSED FH #1	73 PSI
PROPOSED FH #2	78 PSI
PROPOSED FH #3	85 PSI

FIRE FLOW (750 GPM FOR 1 HOUR)

PROPOSED FH #1	45 PSI
PROPOSED FH #2	49 PSI
PROPOSED FH #3	56 PSI

DEPTH FOR MAIN IMPROVEMENTS TO BE NO LESS THAN 36 INCHES EXCEPT WHERE IN CONFLICT WITH ELECTRICAL CROSSINGS, WHEREIN WATER MAINS SHOULD BE A MINIMUM 18 INCHES OF SEPARATION FROM CARROLL ELECTRIC CONDUITS.

NOTES:

EXISTING CITY PLANS SHOW A GATE VALVE PAST THE EXISTING STUB AT THE END OF THE EXISTING 8 INCH MAIN. HOWEVER, THE GATE VALVE COULD NOT BE FIELD LOCATED. CONTRACTOR TO CONFIRM THE ABSENCE OF GATE VALVE BEFORE CONSTRUCTION BEGINS. CONTRACTOR TO COORDINATE WITH BVPOA TO INSTALL PROPOSED GATE VALVE.



NO.	REVISIONS	DESCRIPTION	DATE

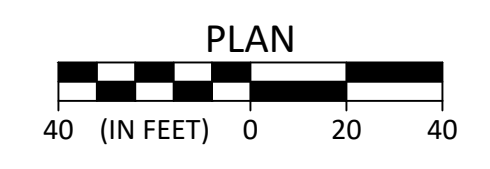
OWL COMMONS

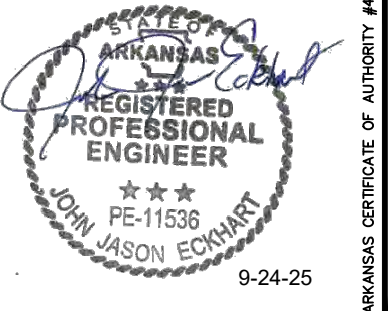
102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-8.0

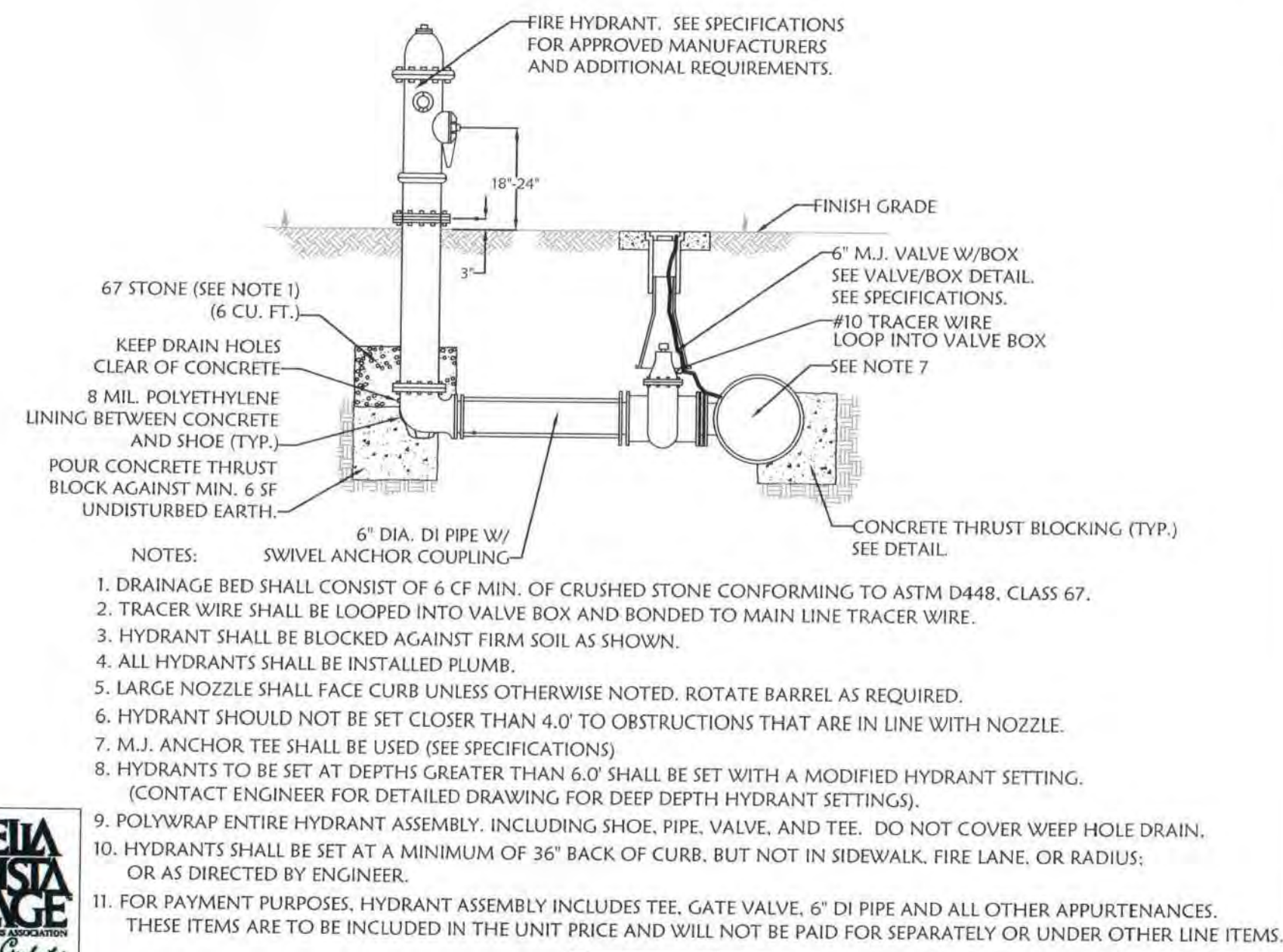
WATER MAIN LAYOUT

PROJECT NUMBER: 230082
DATE: 08/28/2025



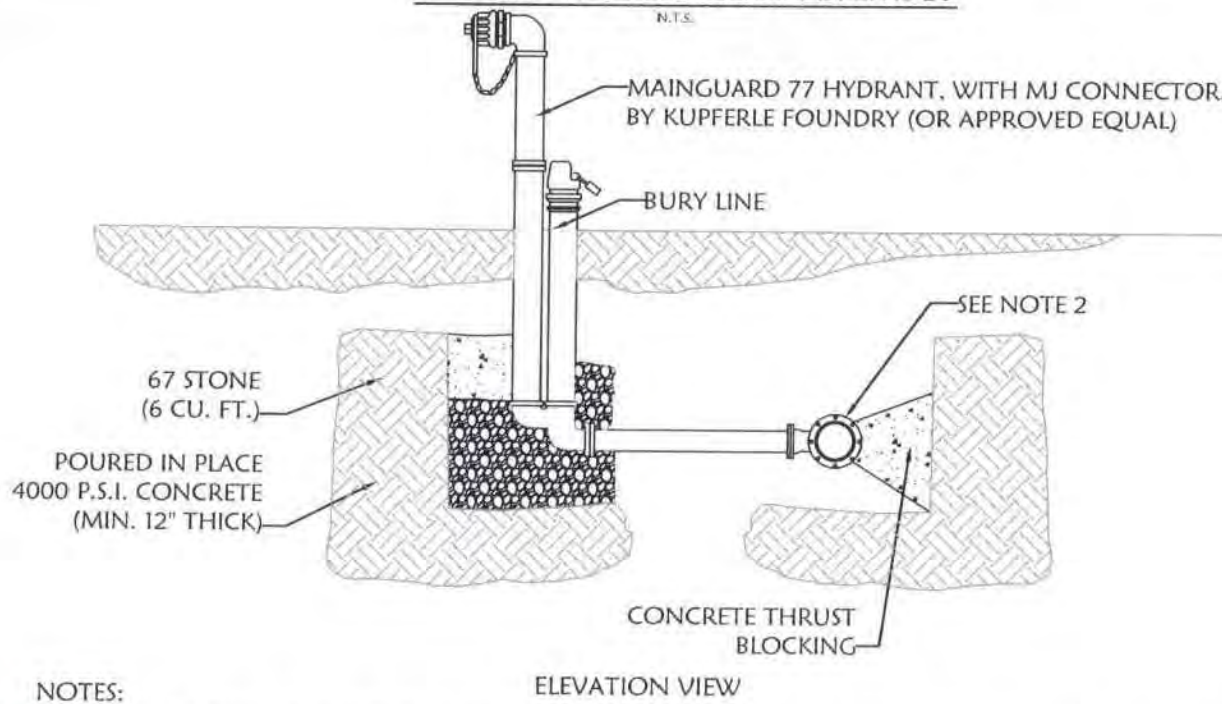


FIRE HYDRANT ASSEMBLY DETAIL



FIRE HYDRANT LAST REVISED: 7/7/2014

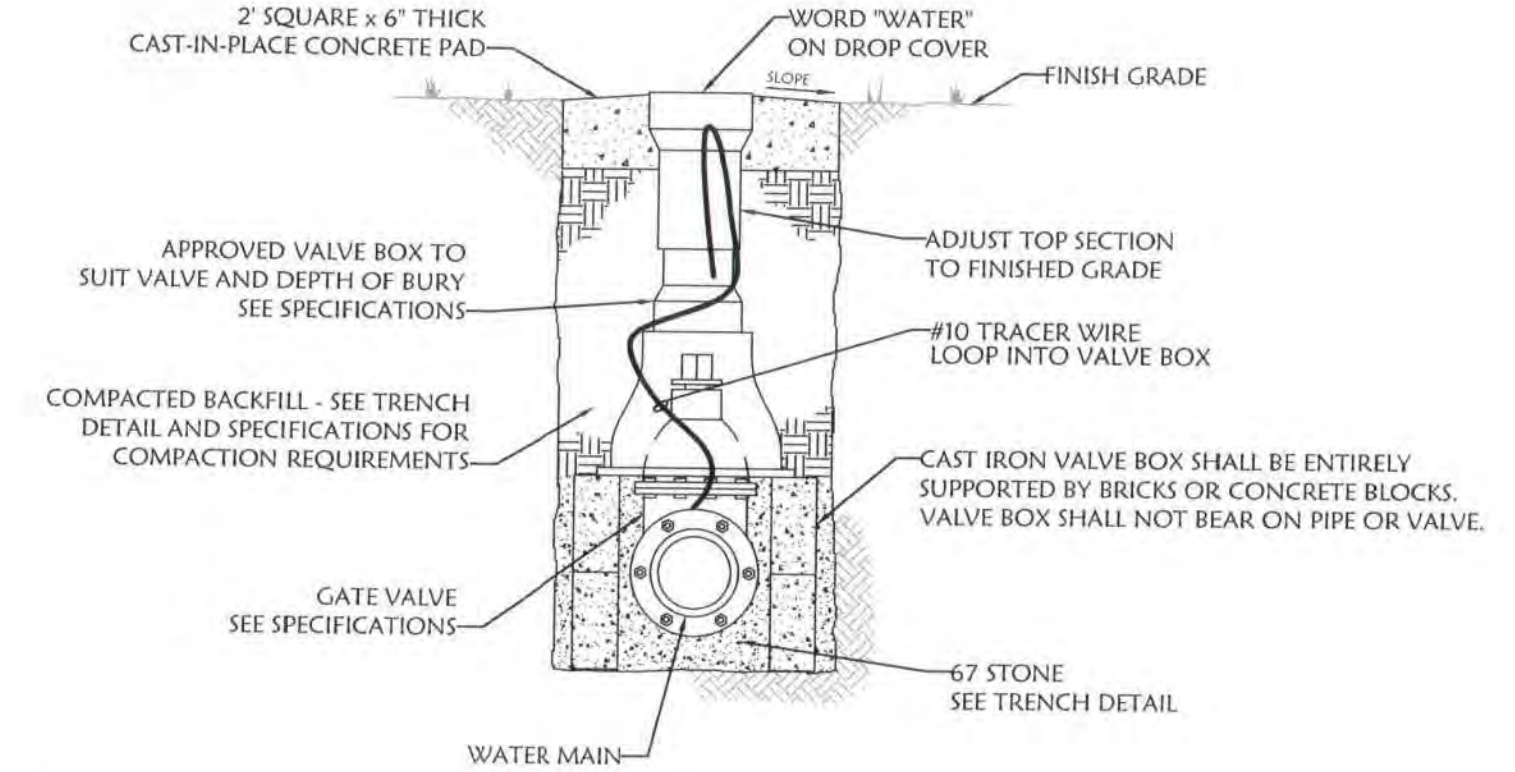
2" FLUSHING HYDRANT ASSEMBLY



- NOTES:
1. DRAINAGE BED SHALL CONSIST OF 8 CU FT CRUSHED STONE CONFORMING TO ASTM D448, CLASS 67.
 2. USE MECHANICAL JOINT FITTINGS WITH RETAINER GLANDS PER SPECIFICATIONS.
 3. HYDRANT SHALL BE BLOCKED AGAINST FIRM SOIL AS SHOWN. DO NOT COVER WEEP HOLE DRAIN.
 4. ALL HYDRANTS SHALL BE INSTALLED PLUMB.
 5. NOZZLE SHALL FACE CURB UNLESS OTHERWISE NOTED. ROTATE BARREL AS REQUIRED.
 6. HYDRANT SHALL NOT BE SET CLOSER THAN 4.0' TO OBSTRUCTIONS THAT ARE IN LINE WITH NOZZLE.
 7. POLYWRAP ENTIRE HYDRANT ASSEMBLY. DO NOT COVER WEEP HOLE DRAIN.

FLUSHING HYDRANT LAST REVISED: 7/7/2014

GATE VALVE AND VALVE BOX INSTALLATION



- NOTES:
1. IF DEPTH OF BURY EXCEEDS 4'. A SELF-LOCKING VALVE EXTENSION SHALL BE REQUIRED. THE EXTENSION STEM SHALL EXTEND TO 2' BELOW THE LID OF THE VALVE BOX. SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
 2. TRACER WIRE SHALL BE LOOPED INTO VALVE BOX AND BONDED TO MAIN LINE TRACER WIRE.
 3. REFER TO SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.

GATE VALVE LAST REVISED: 7/7/2014

NO.	DESCRIPTION	BY	DATE



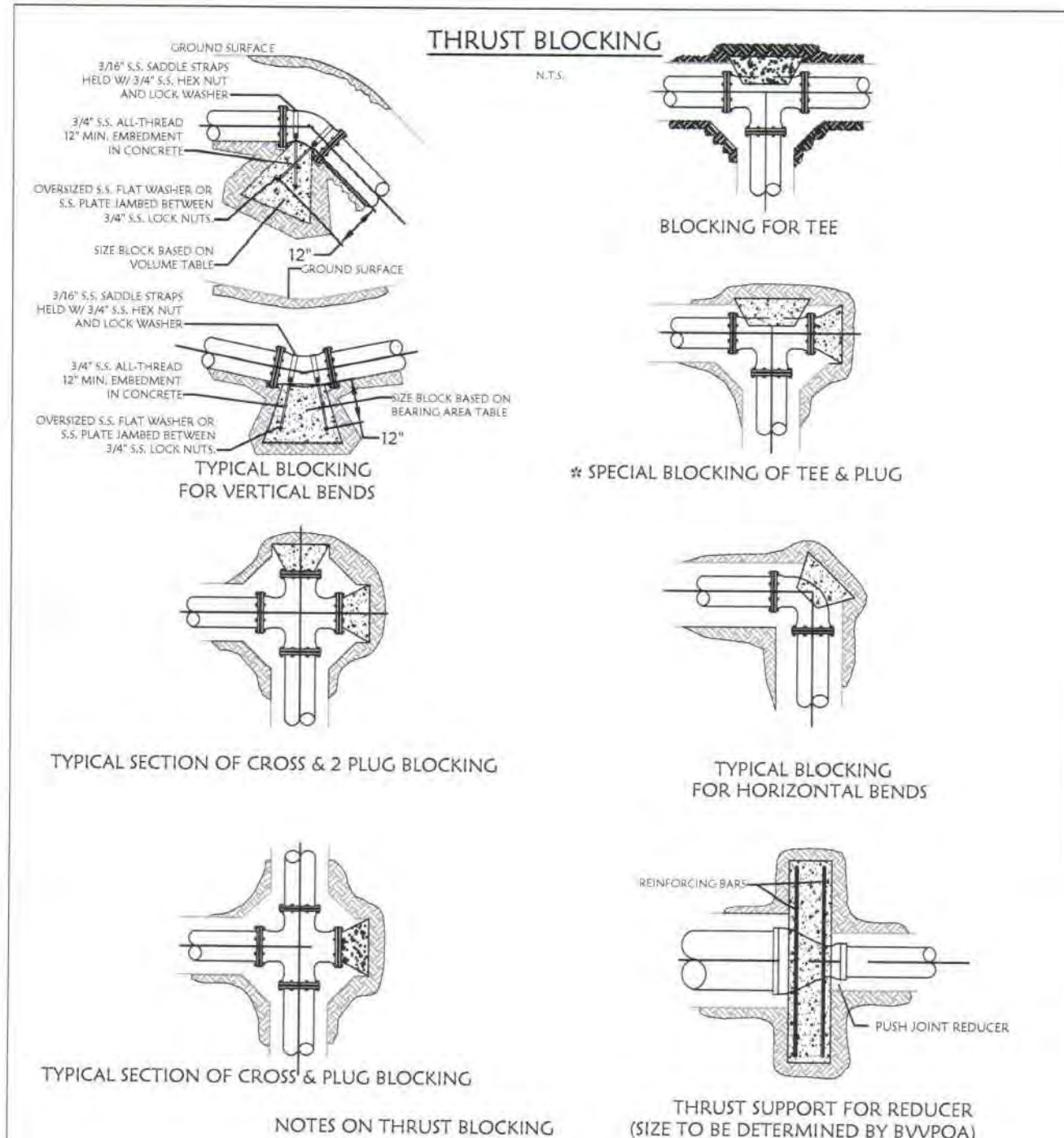
REACTION BACKING TABLE

SIZE	REQUIRED SQ. FT. OF UNDISTURBED EARTH/WALL FOR REACTION BACKING					
	TYPE OF FITTINGS					
	TEE	90°	45°	22 1/2°	11 1/4°	PLUG
2"	1	1	1	1	1	1
3"	2	2	1	1	1	2
4"	3	3	2	1	1	3
6"	5	7	4	2	1	5
8"	9	12	7	4	2	9
12"	19	27	15	8	4	19
16"	34	47	26	13	7	34
18"	42	60	33	17	9	42

SIZE	REQUIRED CU. YD. OF P.C. CONCRETE FOR VERTICAL THRUST BLOCKING*				
	TYPE OF FITTINGS				
	90°	45°	22 1/2°	11 1/4°	
2"	0.4	0.2	0.1	0.1	
3"	0.9	0.5	0.3	0.2	
4"	1.5	0.8	0.9	0.2	
6"	3.3	1.8	0.9	0.5	
8"	5.8	3.2	1.6	0.5	
12"	13.1	7.1	3.6	1.9	
16"	23.2	12.6	6.4	3.3	
18"	29.4	15.9	8.1	4.1	

- NOTES:
1. ALL FITTINGS SHALL BE MECHANICAL JOINTS WITH MEGALUG RETAINER GLANDS.
 2. DO NOT COVER BELLS OR FLANGES WITH CONCRETE.
 3. WRAP ALL FITTINGS WITH POLY WRAP.
 4. BACK ALL TEES ACCORDING TO SIZE OF BRANCH.
 5. BACKING FUTURE LINE EXTENSIONS SHALL BE SUCH THAT LATER REMOVAL IS POSSIBLE.
 6. ALL BENDS WHERE FITTINGS ARE USED, BOTH HORIZONTAL OR VERTICAL, SHALL BE BACKED WITH CONCRETE.
 7. REACTION BACKING TABLE IS BASED ON 300 PSI TEST PRESSURE AND SOIL BEARING PRESSURE OF 2,000 LB/SQ. FT. ADDITIONAL BACKING MAY BE REQUIRED IN SOME AREAS AS REQUIRED BY ENGINEER.
 8. SEE SPECIFICATIONS FOR CONCRETE REQUIREMENTS.
 9. REACTION BACKING TABLE BASED UPON ASSUMED 330 PSI DESIGN PRESSURE (220 PSI STATIC PRESSURE PLUS 110 PSI WATER HAMMER) AND 2,000 PSF SOIL BEARING PRESSURE. ENGINEER SHALL PROVIDE DESIGN CALCULATIONS IF CONDITIONS VARY FROM THOSE ASSUMED OR WHEN REQUIRED BY BVVPOA.

THRUST BLOCK SIZE TABLE LAST REVISED: 7/7/2014

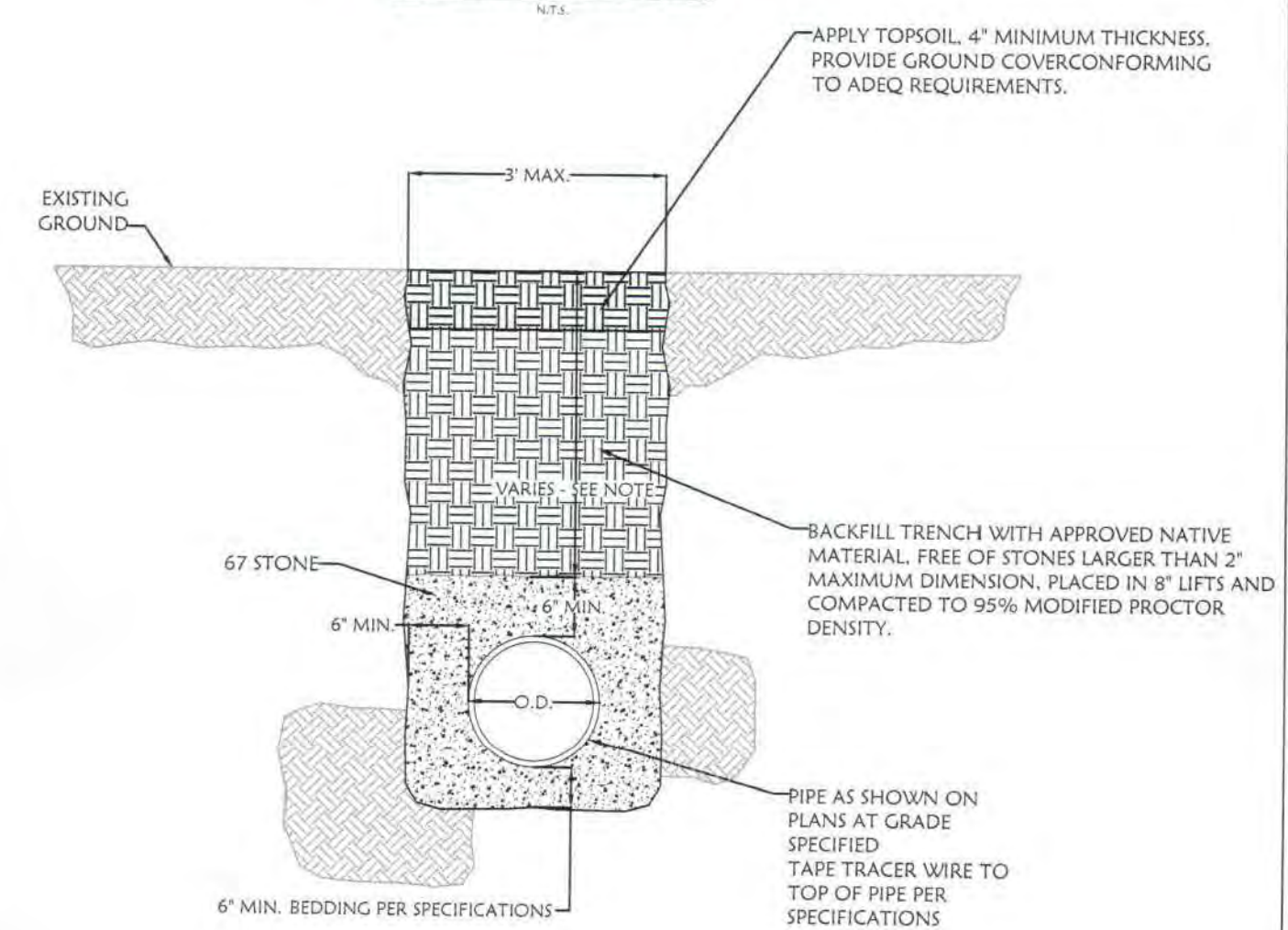


- NOTES ON THRUST BLOCKING:
1. ALL BLOCKING SHALL BE AGAINST UNDISTURBED SOIL AREA (SPECIFIED IN REACTION BACKING TABLE UNLESS SPECIFIED OTHERWISE) USING 4000 PSI CONCRETE.
 2. WHERE SOIL CONDITIONS MAKE IT NECESSARY TO POUR CONCRETE OVER JOINTS, THE ENDS OF THE ADJACENT PIPES MUST HAVE A THRUST BLOCK TO RESIST MOVEMENT OF THESE JOINTS.
 3. WEIGHT CALCULATIONS FOR CERTAIN VERTICAL BENDS SHALL BE BASED ON REACTION BACKING TABLE AS INDICATED.
 4. WHEN BLOCKING AGAINST FITTING, FITTING SHALL BE COVERED WITH POLYETHYLENE TUBING TO PREVENT BONDING OF CONCRETE.
 5. WHERE SHEAR BECOMES A PROBLEM PROPER REINFORCING MUST BE INSTALLED INTO THE BLOCKING.
 6. CLEARANCE SHALL BE A MINIMUM OF 6" BETWEEN PIPE AND OBSTRUCTIONS.
 7. CLEARANCE ON PIPES BELONGING TO OIL/GAS COMPANIES SHALL BE 18" UNLESS SPECIAL PERMISSION IS GIVEN BY THESE COMPANIES.

THRUST BLOCKS LAST REVISED: 7/7/2014



TRENCH SECTION - UNPAVED AREAS (DI, PVC, OR HDPE PIPE)



NOTE: BURY DEPTH FROM TOP OF GROUND TO TOP OF PIPE VARIES. SEE PROFILE FOR BURY DEPTH FOR EACH SECTION OF PIPE. MINIMUM DEPTH OF BURY 36".

TRENCH SECTION - UNPAVED LAST REVISED: 7/7/2014



OWL COMMONS

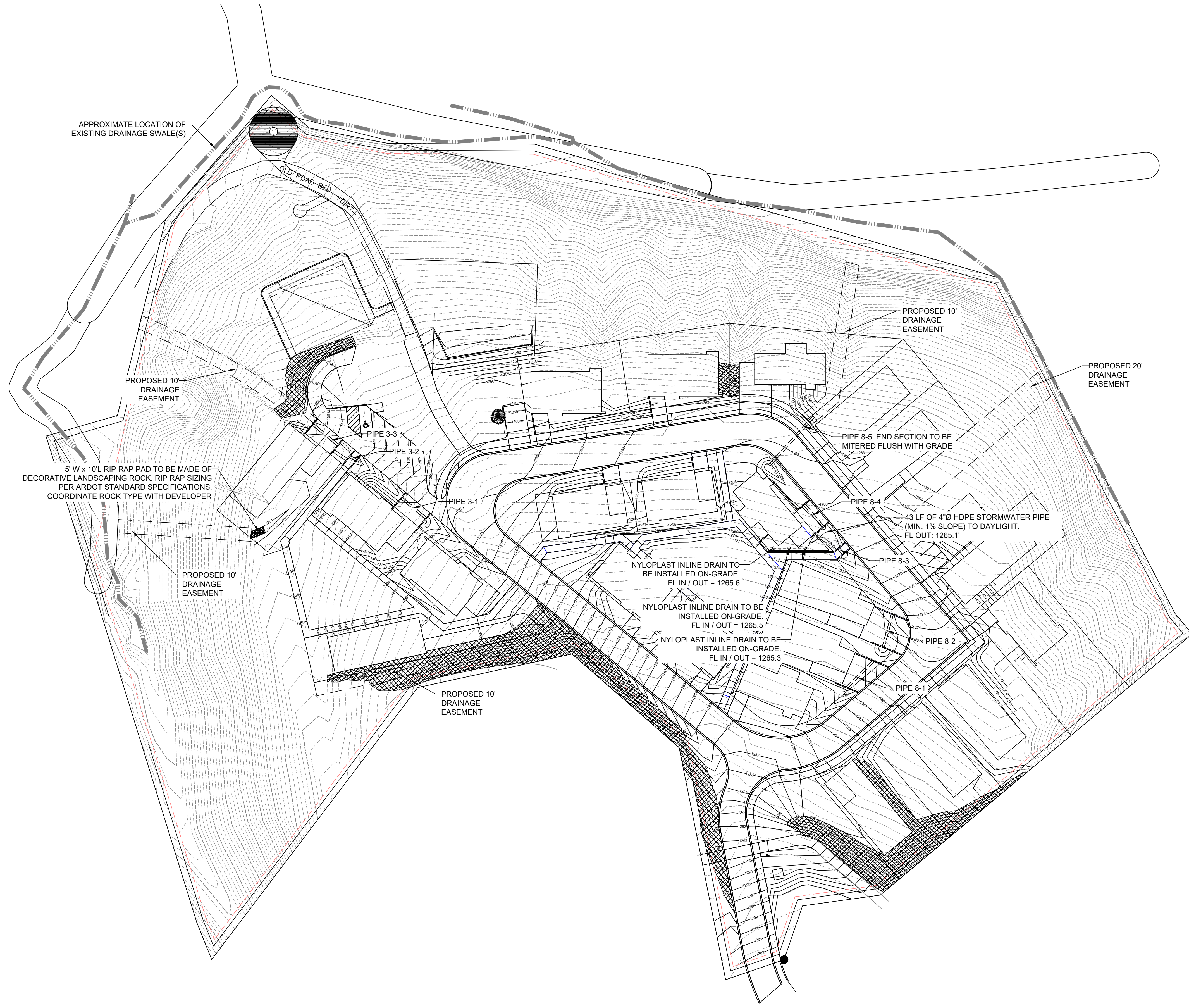
102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-8.1
WATER DETAILS

PROJECT NUMBER:
230082
DATE:
08/28/2025



PHONE: 417-800-2500
1221 OAK ST.
CAPTAGE, MO 64636



NOTES:

SITE DRAINAGE IS TO UTILIZE EXISTING DRAINAGE EASEMENTS ALONG THE NORTHERN EDGE OF THE PROPERTY LIMITS. THE SITE'S TOPOGRAPHY NATURALLY DRAINS TO THESE SWALES INTO LAKE BRITTANY.

INTERNAL DRAINAGE IS EITHER TO BE GRADED TO VEGETATIVE SWALES, DENOTED AS "MAJOR SITE DRAINAGE SWALE" OR SHEET FLOW TO THE EXISTING DRAINAGE EASEMENTS TO TAKE ADVANTAGE OF THE EXISTING VEGETATION TO PREVENT ADDITIONAL SEDIMENTATION FROM FLOWING INTO LAKE BRITTANY.

THE SITE IS NOT EXPECTED TO UTILIZE BELOW GRADE STORMWATER DETENTION STRUCTURES.

CULVERTS BELOW DRIVEWAYS SHOWN ON PLANS ARE PRELIMINARY. PLANS FOR DRIVEWAY CULVERT LOCATION AND SIZING WILL BE PROVIDED AFTER SELECTION OF HOUSE PLANS BY HOMEOWNERS.

NO NATIONAL WETLANDS OR USAGE JURISDICTIONAL AREAS WILL BE IMPACTED BY THE DEVELOPMENT; THEREFORE, NO PERMITTING WILL BE REQUIRED.

THERE IS NO FEMA 1% AEP FLOODPLAIN WITHIN THE PROJECT LIMITS. THE SURROUNDING AREA IS A ZONE X PER MAP NUMBER 05007C0080J, EFF. 09/28/2007. LAKE BRITTANY IS ALSO SHOWN IN ZONE X ON THIS MAP.



LEGEND

- NYLOPLAST INLINE DRAIN
- 4"Ø HDPE PIPE
- 18"Ø RCP PIPE
- PROPOSED STORMWATER EASEMENTS
- ▨ TURF REINFORCEMENT MAT

PIPE INFORMATION					
ID #	LENGTH	SLOPE	FL UP	FL DN	DIA.
3-1	23.3'	5.58%	1257.7'	1256.4'	18"
3-2	5.14'	1.95%	1250.7'	1250.6'	18"
3-3	23.8'	6.30%	1250.5'	1249.0'	18"
8-1	29.0'	6.39%	1278.75'	1276.9'	18"
8-2	27.0'	6.49%	1274.75'	1273.0'	18"
8-3	6.33'	1.58%	1266.0'	1265.9'	18"
8-4	22.7'	2.21%	1264.8'	1264.2'	18"
8-5	48.8'	5.74%	1262.8'	1260.0'	18"



NO.	DESCRIPTION	BY	DATE

OWL COMMONS

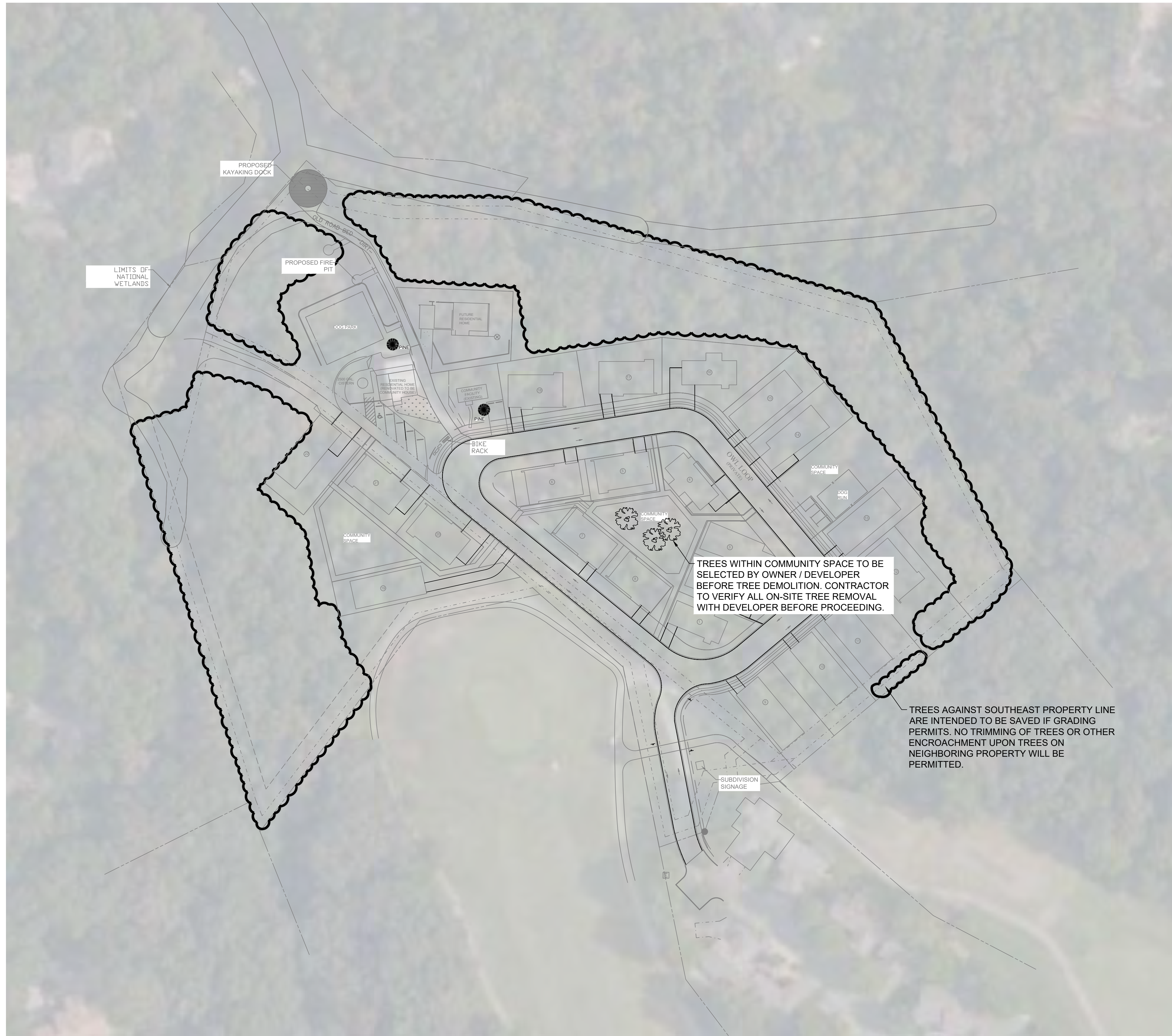
102 FAIRWAY DRIVE
BELLA VISTA, ARKANSAS 72714

C-9.0
DRAINAGE PLAN

PROJECT NUMBER:
230082
DATE:
08/28/2025



PHONE: 417-800-2500
1221 OAK ST.
CARTHAGE, MO 64836



- LEGEND**
- PINE TREE (TO BE RETAINED)
 - ⊗ MISC. TREES (TO BE RETAINED)
 - TREE COVERED AREAS (TO BE RETAINED. REFER TO GRADING PLAN FOR EXACT LIMITS)

NOTES:

THE SITE IS CONSIDERED TO BE HEAVILY WOODED WITH A NUMBER OF TREES OVER 6" OR MORE IN SIZE. TREES DESIGNATED ABOVE ARE MARKED OUT AS THOSE PLANNED TO BE RETAINED POST-CONSTRUCTION.

SITE DESIGN AND CONSTRUCTION ARE TO BE DESIGNED IN SUCH A WAY TO LIMIT THE CUTTING DOWN OF AS MANY TREES AS POSSIBLE. FINAL PLANS WILL SHOW LIMITS OF GRADING.

MISC. TREES AS SHOWN ARE WITHIN A HEAVILY WOODED AREA. EXACT TREES WHICH REMAIN MAY VARY.

APPROXIMATE ACREAGE ALONG NORTH PROPERTY LINES TO BE LEFT UNDISTURBED = 2.75 ACRES



NO.	DESCRIPTION	BY	DATE

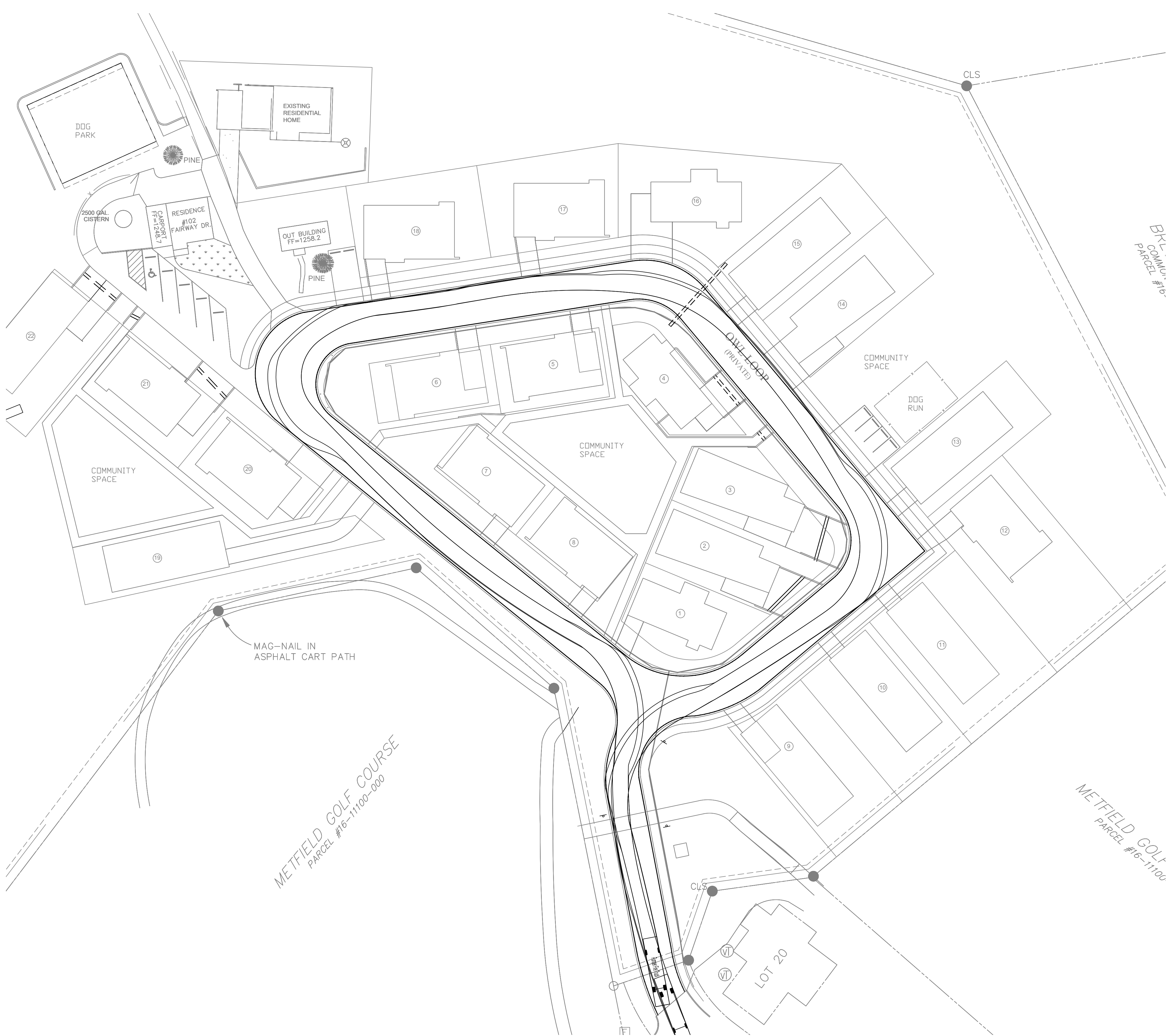
OWL COMMONS

102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-10.0
 BUFFERING &
 SCREENING

PROJECT NUMBER:
 230082
 DATE:
 08/28/2025





LEGEND

— FIRE TRUCK PATH

PLAN

30 (IN FEET) 0 15 30



NO.	REVISIONS	DESCRIPTION	BY	DATE

OWL COMMONS

FIRE TRUCK TURNAROUND PATH

102 FAIRWAY DRIVE
 BELLA VISTA, ARKANSAS 72714

C-11.0

FIRE TRUCK TURNAROUND PATH

PROJECT NUMBER: 230082
 DATE: 08/28/2025

ZANEVAN ENGINEERING

PHONE: 417-800-2500
 1221 OAK ST.
 CARPAGE, MO 64636